

Planning Committee Agenda



To: Councillor Toni Letts (Chair)
Councillor Paul Scott (Vice-Chair)
Councillors Muhammad Ali, Sherwan Chowdhury, Chris Clark, Joy Prince,
Jason Perry, Scott Roche, Ian Parker and Gareth Streeter

Reserve Members: Felicity Flynn, Bernadette Khan, Clive Fraser, Leila Ben-Hassel, Helen Redfern, Michael Neal, Badsha Quadir, Jan Buttinger and Andrew Pelling

A meeting of the **Planning Committee** which you are hereby summoned to attend, will be held on **Thursday, 21 November 2019** at **6.00 pm** in **Council Chamber, Town Hall, Katharine Street, Croydon CR0 1NX**

JACQUELINE HARRIS BAKER
Council Solicitor and Monitoring Officer
London Borough of Croydon
Bernard Weatherill House
8 Mint Walk, Croydon CR0 1EA

Michelle Ossei-Gerning
020 8726 6000 x84246
michelle.gerning@croydon.gov.uk
www.croydon.gov.uk/meetings
Wednesday, 13 November 2019

Members of the public are welcome to attend this meeting.

If you require any assistance, please contact the person detailed above, on the right-hand side.

To register a request to speak, please either e-mail

Democratic.Services@croydon.gov.uk or phone the number above by 4pm on the Tuesday before the meeting.

THIS MEETING WILL BE WEBCAST LIVE - Click on link to view:

<http://webcasting.croydon.gov.uk>

N.B This meeting will be paperless. The agenda can be accessed online at

www.croydon.gov.uk/meetings

AGENDA – PART A

1. Apologies for absence

To receive any apologies for absence from any members of the Committee.

2. Minutes of Previous Meeting (Pages 5 - 10)

To approve the minutes of the meeting held on Wednesday 6 November 2019 as an accurate record.

3. Disclosure of Interest

In accordance with the Council's Code of Conduct and the statutory provisions of the Localism Act, Members and co-opted Members of the Council are reminded that it is a requirement to register disclosable pecuniary interests (DPIs) and gifts and hospitality to the value of which exceeds £50 or multiple gifts and/or instances of hospitality with a cumulative value of £50 or more when received from a single donor within a rolling twelve month period. In addition, Members and co-opted Members are reminded that unless their disclosable pecuniary interest is registered on the register of interests or is the subject of a pending notification to the Monitoring Officer, they are required to disclose those disclosable pecuniary interests at the meeting. This should be done by completing the Disclosure of Interest form and handing it to the Democratic Services representative at the start of the meeting. The Chair will then invite Members to make their disclosure orally at the commencement of Agenda item 3. Completed disclosure forms will be provided to the Monitoring Officer for inclusion on the Register of Members' Interests.

4. Urgent Business (if any)

To receive notice of any business not on the agenda which in the opinion of the Chair, by reason of special circumstances, be considered as a matter of urgency.

5. Development presentations (Pages 11 - 12)

To receive the following presentations on a proposed development:

5.1 19/02842/PRE Croydon College Annexe, College Road, Croydon, CR9 1DX (Pages 13 - 36)

Mixed redevelopment of the site to provide circa 90 flats and a 'Cultural and creative industries enterprise centre'.

Ward: Fairfield

6. Planning applications for decision (Pages 37 - 40)

To consider the accompanying reports by the Director of Planning & Strategic Transport:

6.1 19/02678/FUL 64 to 74 Whytecliffe Road North, Purley, CR8 2AR (Pages 41 - 70)

Demolition of three pairs of semi-detached houses, erection of a part 3/part 5/part 6 storey building with basement to provide 39 residential units, with associated terraces, disabled car parking spaces, amenity spaces and landscaping.

Ward: Purley Oaks and Riddlesdown
Recommendation: Grant permission

6.2 18/05856/FUL Station Yard, 56 Brigstock Road (Pages 71 - 100)

Demolition of existing builders yard buildings, erection of a block comprising a 6 storey building and a part 7, 8 and 9 storey building comprising 58 residential units (26 x 1 bed, 9 x 2 bed and 23 x 3 bed apartments) and two commercial units (Use Class B1c) along with the provision of associated off-street parking and refuse and cycle storage.

Ward: Bensham Manor
Recommendation: Grant permission

6.3 19/03074/FUL 5 Highland Road, Purley, CR8 2HS (Pages 101 - 120)

Demolition of existing detached house and detached garage and replacement with 9 new apartments in a single block of four / five storeys with parking to the front.

Ward: Kenley
Recommendation: Grant permission

6.4 19/03385/FUL 34 Caterham Drive, Coulsdon, CR5 1JF (Pages 121 - 142)

Demolition of existing dwelling and garage and erection of a three storey building comprising 6 flats (3 x 3 bed, 1 x 2 bed and 2 x 1 bed), 3 parking spaces, communal amenity space and cycle / refuse / recycle storage.

Ward: Old Coulsdon
Recommendation: Grant permission

6.5 19/03410/FUL 22 Purley Knoll, Purley, CR8 3AE
(Pages 143 - 166)

Demolition of existing buildings; Erection of 7no. apartments and 2no. dwellings and associated works.

Ward: Purley and Woodcote
Recommendation: Grant permission

7. Items referred by Planning Sub-Committee

To consider any item(s) referred by a previous meeting of the Planning Sub-Committee to this Committee for consideration and determination:

There are none.

8. Other planning matters (Pages 167 - 168)

To consider the accompanying report by the Director of Planning & Strategic Transport:

8.1 Weekly Planning Decisions (Pages 169 - 226)

This report provides a list of cases determined (since the last Planning Committee) providing details of the site and description of development (by Ward), whether the case was determined by officers under delegated powers or by Planning Committee/Sub Committee and the outcome (refusal/approval).

9. Exclusion of the Press & Public

The following motion is to be moved and seconded where it is proposed to exclude the press and public from the remainder of a meeting:

"That, under Section 100A(4) of the Local Government Act, 1972, the press and public be excluded from the meeting for the following items of business on the grounds that it involves the likely disclosure of exempt information falling within those paragraphs indicated in Part 1 of Schedule 12A of the Local Government Act 1972, as amended."

Planning Committee

Meeting of Croydon Council's Planning Committee held on Wednesday, 6 November 2019 at 6.00 pm in Council Chamber, Town Hall, Katharine Street, Croydon CR0 1NX

This meeting was Webcast – and is available to view via the Council's Web Site

MINUTES

Present: Councillor Toni Letts (Chair);
Councillor Paul Scott (Vice-Chair);
Councillors Muhammad Ali, Joy Prince, Jason Perry, Scott Roche, Bernadette Khan (In place of Chris Clark), Leila Ben-Hassel (In place of Sherwan Chowdhury), Helen Redfern (In place of Gareth Streeter) and Badsha Quadir (In place of Ian Parker)

Also Present: Councillor Simon Hoar, Tim Pollard and Sue Bennett

Apologies: Councillors Scott Roche for lateness

PART A

At 6.04pm Councillor Roche arrived at the Meeting

246/19 **Minutes of Previous Meeting**

RESOLVED that the minutes of the meeting held on Thursday 24 October 2019 be signed as a correct record.

247/19 **Disclosure of Interest**

There were no disclosures of a pecuniary interest not already registered.

248/19 **Urgent Business (if any)**

There was none.

249/19 **Development presentations**

250/19 **19/02988/PRE 443A Brighton Road, South Croydon, CR2 6EU**

Proposed redevelopment of the site to provide a residential led, mixed use development comprising 75 residential units (C3) and 338sqm GIA of flexible

commercial space (B1b, B1c and D1), with building heights ranging between 4 and 7 storeys, associated parking and landscaping, and all necessary ancillary and enabling works.

Ward: Purley Oaks and Riddlesdown

Chris Leveret and Alex Giles from DLA Architect, Nick Lawrence from the Developer Age Group and Planning Consultant John Mumby from ICU Projects, attended to give a presentation and respond to Members' questions and issues raised for further consideration prior to submission of a planning application.

The main issues raised at this meeting were as follows:

Height – A few Members supported an extra floor rising to 8 storeys on the corner to improve the appearance of the building particularly if this could improve the affordable offer on site. However other Members were not in favour of the extra increase in height.

Design and Massing – Members were supportive for the greater use of materials to help break the massing of the facade. Some concern around the use of red brick.

Parking – Members were undecided on whether the quantum of parking was enough. Though there was consensus that should there be more car parking space on the site, it should not compromise the design of the landscaping/communal garden. It should be demonstrated that vehicles can service the commercial units without affecting the flow of traffic on Brighton Road.

Commercial Space – Members welcomed the commercial space which would create an active frontage but want to be assured that there would be a demand for it.

Three bed units – Members felt that there should be more three bedroom units in the scheme and noted that none of the proposed three bed units are proposed to be affordable.

Windows – Members would like to see improvements to the south west frontage windows by which they could be made larger. Members were also concerned about the overlooking of surrounding residents in relation to the design of balconies to be considered.

Winter Gardens – Members welcomed the winter gardens but noted that the majority were accessed from bedrooms and it would be better if they were accessed from living areas.

Air Quality – Members noted that the location did not have the best air quality and welcomed the winter gardens. Members would also like to see other

design techniques which could be used to improve air quality for the occupants.

Landscaping – Members highlighted the importance that the correct species were specified so they survive in the landscape court yard as some areas of the site do not get any daylight.

Colonnades – Members were concerned that the colonnade would create a security issue.

Ward Member Councillor Simon Hoar was invited to share his local viewpoint on the development presentation.

The Chair thanked the applicants for their presentation, and looked forward to their application returning to the Committee at a later stage.

251/19 **Planning applications for decision**

252/19 **19/00131/FUL 17 Orchard Avenue CR0 8UB**

Demolition of existing detached house erection of three-storey building with further accommodation in roof space comprising 3 x 1 bedroom flat, 4 x 2-bedroom flats and 1 x 3 bedroom flat, formation of vehicular access and provision of 4 associated parking spaces and refuse storage.

Ward: Shirley North

The officers presented details of the planning application and officers responded to questions for clarification.

Ms Susannah Angold spoke against the application

Mr Alan Gunne-Jones spoke in support of the application.

Ward Member Cllr Bennett spoke on behalf of the referring Ward Member Councillor Richard Chatterjee against the application

Councillor Scott proposed a motion for **APPROVAL** of the application based on the officer's recommendation. Councillor Ali seconded the motion. There was a request for a wildlife area in the scheme to be part of the landscape condition. There was a suggestive request for a green roof to be considered.

Councillor Roche proposed a motion to **REFUSE** the application on the grounds of insufficient parking, intensity on the red route and insufficient private amenity space on the upper units. Councillor Perry seconded the motion.

The motion to approve was put forward to the vote and carried with six Members voting in favour and four Members voting against. The second motion to refuse therefore fell.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of 17 Orchard Avenue, CR0 8UB.

253/19 **19/03203/FUL 116 Riddlesdown Road, Purley, CR8 1DD**

Demolition of existing 4 bedroom, two storey detached house and the erection of new 4 storey building including basement and roof accommodation to provide 8 units including associated access, parking, landscaping, as well as internal refuse and cycle storage.

Ward: Purley Oaks and Riddlesdown

The officers presented details of the planning application and officers responded to questions for clarification.

Ms Nimmi Channa and Mr Adam Doyle spoke against the application.

Referring Ward Member Councillor Hoar spoke against the application.

Councillor Perry proposed a motion to **REFUSE** the application on the grounds of over development by extent of its size and massing, not compliant with the London Plan parking standards. Councillor Roche seconded the motion.

Councillor Letts proposed a motion to **APPROVE** the application based on the officer's recommendation. Councillor Ali seconded the motion.

The motion to refuse was put forward to the vote and fell with four Members voting in favour and six Members voting against.

The motion to approve was put forward to the vote and carried with five Members voting in favour, four Members voting against and one Member abstaining their vote.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of 116 Riddlesdown Road, Purley, CR8 1DD.

254/19 **19/03118/FUL 3 West Hill, South Croydon, CR2 0SB**

Demolition of existing 2 storey detached house & garage. Erection of a four storey flatted block comprising 9 apartments including associated communal amenity space, landscaping, parking, access, cycle and refuse storage.

Ward: Sanderstead

The officers presented details of the planning application and there was no questions for clarifications.

Mr Simon David spoke against the application.

Mr Patrick Stroud spoke in support of the application.

Ward Member Cllr Tim Pollard spoke on behalf of the referring Ward Member Councillor Yvette Hopley against the application.

Councillor Perry proposed a motion to **REFUSE** the application on the grounds of over development by dense of size and massing, design out of character with local street scene contrary to DM10.1 A, B and C, loss of amenity to adjoining occupiers and insufficient parking contrary to London Plan Policy 6.13. Councillor Redfern seconded the motion.

Councillor Scott proposed a motion to **REFUSE** the application on the grounds of the quality of design not in keeping with the character of the area. Councillor Ali seconded the motion.

The motion to refuse (on grounds of overdevelopment, out of character, loss of amenity and insufficient parking) was put forward to the vote and fell with four Members voting in favour, five Members voting against and one Member abstaining their vote.

The motion to refuse (on grounds of design out of character) was put forward to the vote and carried with nine Members voting in favour and one Member abstaining their vote.

The Committee therefore **RESOLVED** to **REFUSE** (on grounds of design out of character) the application for the development of 3 West Hill, South Croydon, CR2 0SB.

255/19 Items referred by Planning Sub-Committee

There were none.

256/19 Other planning matters

257/19 Weekly Planning Decisions and Performance

The report was received for information.

258/19 Planning Appeal Decisions (October 2019)

The report was received for information.

The meeting ended at 9.32 pm

Signed:

.....

Date:

.....

PLANNING COMMITTEE AGENDA

PART 5: Development Presentations

1 INTRODUCTION

- 1.1 This part of the agenda is for the committee to receive presentations on proposed developments, including when they are at the pre-application stage.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 The following information and advice applies to all those reports.

2 ADVICE TO MEMBERS

- 2.1 These proposed developments are being reported to committee to enable members of the committee to view them at an early stage and to comment upon them. They do not constitute applications for planning permission at this stage and any comments made are provisional and subject to full consideration of any subsequent application and the comments received as a result of consultation, publicity and notification.
- 2.2 Members will need to pay careful attention to the probity rules around predisposition, predetermination and bias (set out in the Planning Code of Good Practice Part 5.G of the Council's Constitution). Failure to do so may mean that the Councillor will need to withdraw from the meeting for any subsequent application when it is considered.

3 FURTHER INFORMATION

- 3.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

4 PUBLIC SPEAKING

- 4.1 The Council's constitution only provides for public speaking rights for those applications being reported to Committee in the "Planning Applications for Decision" part of the agenda. Therefore reports on this part of the agenda do not attract public speaking rights.

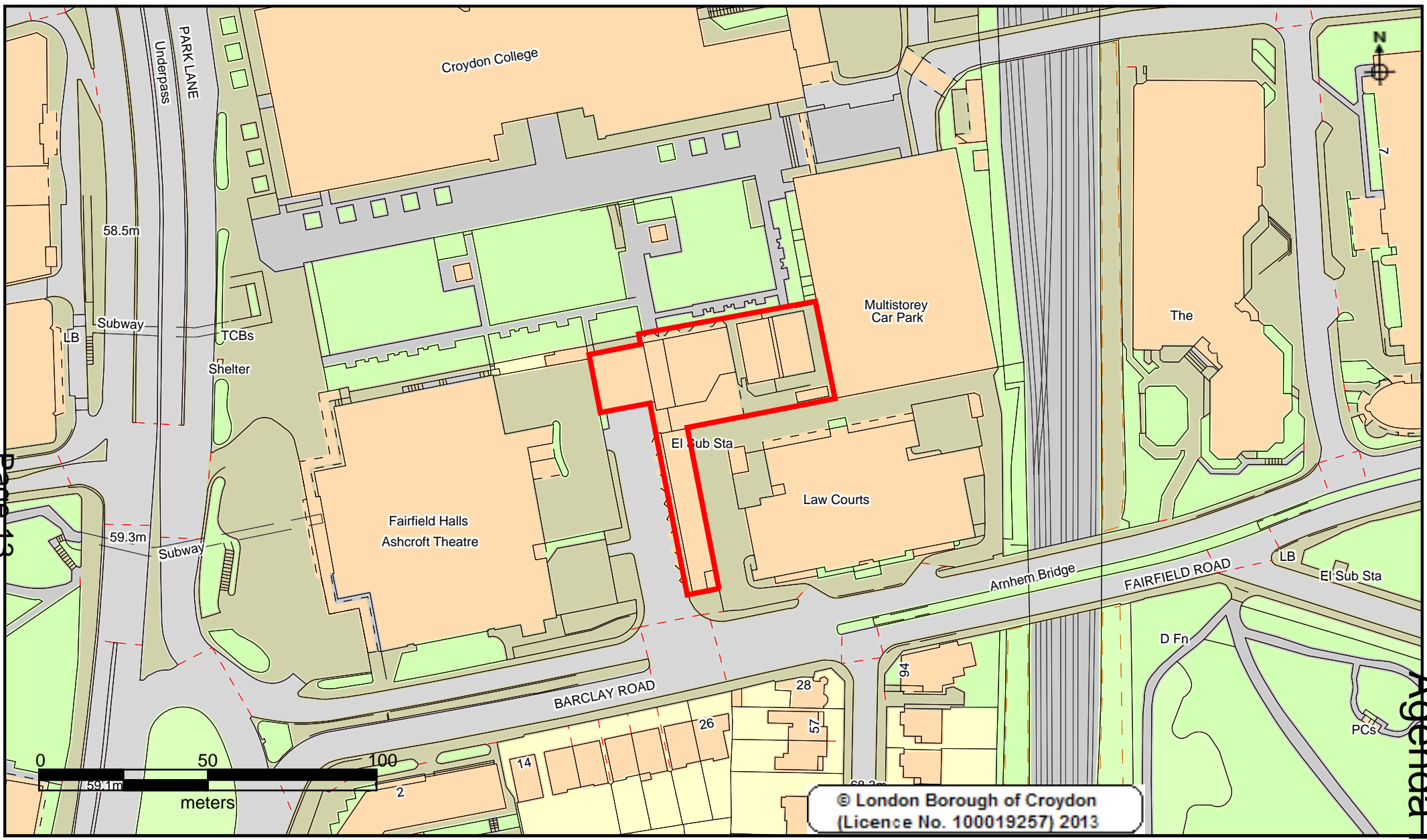
5 BACKGROUND DOCUMENTS

- 5.1 For further information about the background papers used in the drafting of the reports in part 8 contact Mr P Mills (020 8760 5419).

6 RECOMMENDATION

- 6.1 The Committee is not required to make any decisions with respect to the reports on this part of the agenda. The attached reports are presented as background information.

This page is intentionally left blank



This page is intentionally left blank

1 DETAILS OF THE DEVELOPMENT

Ref: 19/02842/PRE
Location: Croydon College Annexe, College Road, Croydon, CR9 1DX
Ward: Fairfield
Description: Mixed redevelopment of the site to provide circa 90 flats and a 'Cultural and creative industries enterprise centre'
Applicant: Stonegate Homes
Agent: Montagu Evans
Case Officer: Katy Marks

2 EXECUTIVE SUMMARY

- 2.1 The scheme currently proposes the redevelopment of the Croydon College Annexe comprising a part 4, part 12 storey building to the northern section of the site providing a Creative and Cultural Industries Enterprise Centre (currently being designed for use as artist studios) and 90 residential units, together with a pedestrian route through the site connecting College Green (Fair Field) to Barclay Road.
- 2.2 The application site was previously included as part of the detailed consent element of the 2017 hybrid planning permission for the whole Fairfield Masterplan area. The site and the adjacent Fairfield Homes site are now coming forward independently as they are in separate ownership.
- 2.3 The scheme has evolved through a series of pre-application meetings and has been reviewed by planning officers and presented to the Place Review Panel (PRP). The current scheme has evolved since PRP to respond to specific feedback concerning the layout and massing of the proposals. Pre-application discussions have focused on appropriate height and massing, impact upon the locally designated Fairfield Halls, the provision and design of the public route through the site, type and quality of accommodation, relationship with adjacent development sites and the design and layout of the enterprise centre.
- 2.4 The views of Members are sought on the proposals, with particular regard to the following key issues:
- Route and Public Realm*
- 2.5 One of the key aims of the Fair Field Masterplan was to improve pedestrian routes within the Masterplan area and the wider context. Facilitating a new pedestrian link through this site to Barclay Road (to the south) forms part of these objectives.
- 2.6 The scheme has been developed to provide a level (step free) route from Barclay Road to College Green which will connect into the proposed levels for the Fair Field public realm scheme which is currently at an initial design stage. The

applicant has worked hard to improve the legibility, width and pedestrian experience of this space; particularly where it would pass through the building via an under-croft. Members' views are sought on the applicant's approach to this route, the public realm and amenity space layout within the site and the way in which the scheme engages with adjacent public realm.

Cultural and Creative Industries Enterprise Centre

- 2.7 The site allocation includes a cultural and creative industries enterprise centre. The applicant has sought to design space at ground and first floor to cater for artist studios and gallery/café space. The design has evolved through discussions with the Council's Cultural Team and an existing artist studio provider (Artist Studio Company). Members' views are sought on the design and layout of the proposed cultural and creative industries enterprise centre.

Design, views, layout and quality of accommodation

- 2.8 Members' views are sought on the site layout and architectural design of the scheme, including its relationship to Fairfield Halls and College Green. Members' views are also sought on the residential layouts including the access and core arrangements, flat layouts and orientations.

3 LOCATION DETAILS

Site and Surroundings

- 3.1 The site comprises the former Croydon School of Art building located to the rear of Fairfield Halls. The building is of a T-shaped form, with the main bulk positioned along the northern boundary facing onto College Green. To the rear the site has an extensive north-south spine which faces onto the service road ramp which leads from Barclay Road (Barclay Road ramp). The building on the site is now derelict and has been mostly empty since the school was consolidated into the main Croydon College building back in in 2012. The building was previously accessed at first floor level via steps from College Green and had a secondary access onto Barclay Road. The building was serviced via the ramp with a basement car park to the eastern part of the site accessed under an archway below the building.



View of existing northern elevation (showing College Green and Fairfield Halls to the right of the photo and the Fairfield Homes development site to the left with the Magistrates Court behind)

- 3.2 The site is bounded to the north by the edge of College Green and the Fairfield Homes development site. To the east it is bounded by the Magistrates Court and to the west, the site partially abuts the Fairfield Halls service yard and otherwise sits alongside the Barclay Road ramp. The Barclay Road ramp provides access to the public car park under College Green (the western wing of the existing building currently straddles the access point to the car park) and servicing access to the site and Fairfield Halls.



Aerial view of southern elevation (including access under the existing building to the public car park from the Barclay Road Ramp)

- 3.3 The land levels are complex, with Barclay Road sitting at a slightly higher level to the existing 'ground level' for College Green which sits on a podium above a basement car park. The Barclay Road ramp drops towards the entrance of the basement car park (from roughly 61 AOD at Barclay Road to about 55 AOD at the entrance of the car park) and the northern part of the site has a basement at roughly this level. The land level for College Green may be subject to some change as part of anticipated future landscaping to provide a world class public space.
- 3.4 The surrounding area is a mixture of community and cultural uses which form part of the Fair Field Masterplan Area. To the south, the nearby residential development is mainly 2-3 storeys and forms part of the Chatsworth Road Conservation Area.
- 3.5 The site has a number of designations in the Croydon Local Plan 2018:
- Allocated for residential use (20-56 indicative units), community use and a Creative and Cultural Industries Enterprise Centre (Site Allocation 294)
 - Located within the Croydon Opportunity Area

- Located within the Croydon Metropolitan Centre
- Located within the Fair Field Masterplan area

3.6 The Public Transport Accessibility Level (PTAL) of the site is 6b, which is the highest level of accessibility. Barclay Road is a TfL Classified Road and a Red Route.

3.7 The site is located with Flood Risk Zone 1, there is one record of sewer flooding on the site.

Background: Fair Field Masterplan, Hybrid Permission and Emerging Context

3.8 The Fair Field Masterplan covers the area bounded by George Street, Park Lane, Barclay Road and the railway line and provides a framework for the redevelopment of the area as Croydon’s cultural and learning quarter; focussing on a lively and sustainable mix of residential, cultural, educational and commercial uses as well as a well-connected and high quality public realm.

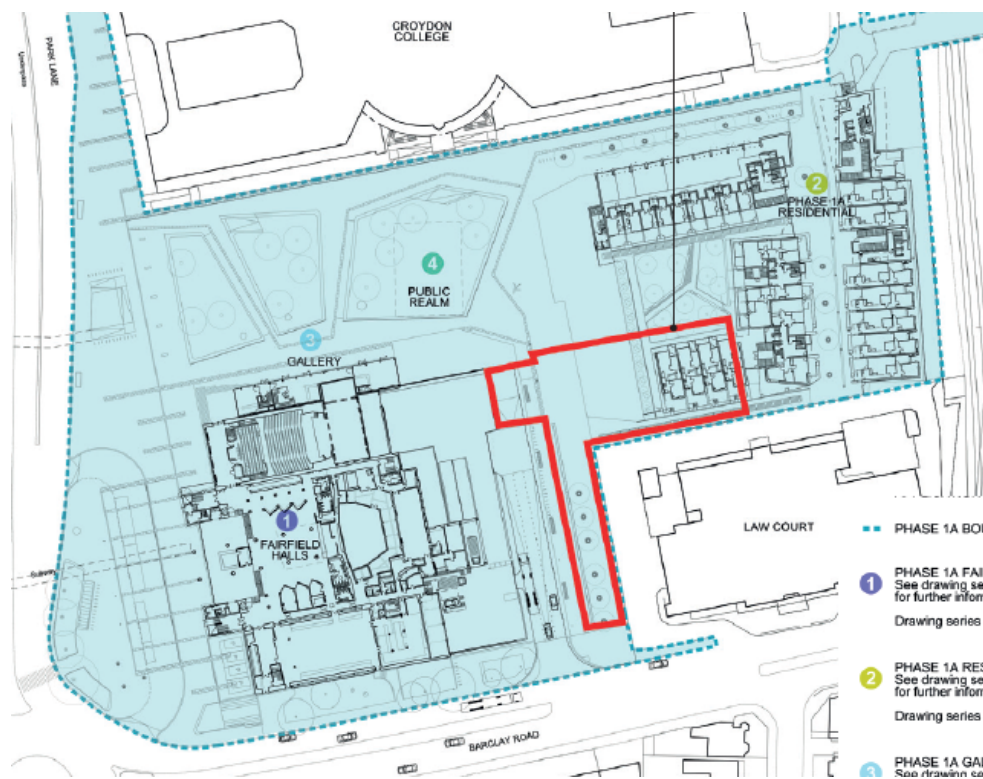
3.9 Key aims of the Masterplan which are relevant to this scheme include:

- Increased accessibility, legibility and activity to support enhanced potential for development sites
- An animated and well used public realm which complements the surrounding spaces
- Better pedestrian connections within the Masterplan area and to the surrounding area including a route from College Green to the south towards Barclay Road and Park Hill Park



Illustrative render of the Fair Field Masterplan

- 3.10 The hybrid permission for the whole Masterplan area (listed in the planning history below) included the site which was at the time owned by Croydon College. As part of the hybrid scheme, the existing building was to be demolished to make way for a generous pedestrian route between College Green and Barclay Road and the remodelling of the Barclay Road ramp. In this scheme, the eastern part of the Annexe site was expected to provide a residential building of 4 storeys (above podium level) which was intended to form the southern edge of a residential courtyard. Additionally, a new college building was expected to be built which would have straddled the pedestrian route, siting to the north and west of the current site.
- 3.11 Since the hybrid permission was consented, the Annexe has been sold by the College and this scheme is being brought forward independently to the rest of the hybrid permission. It should be also be noted that the remaining part of the previous residential scheme (sited to the north and east of the site) is now the subject of a new planning application for a mixed use scheme of five building providing 421 flats and flexible commercial space, public realm, landscaping and basement car parking. This scheme includes a residential courtyard immediately to the north of the site as was envisaged under the hybrid permission.



Plan showing location of College Annexe Site (red line) and relationship with the development associated with the Hybrid permission

Planning History

- 3.12 An application for refurbishment and conversion of the existing building was withdrawn earlier in the year due to concerns raised by officers specifically with regards to the quality of accommodation and quality and safety of the proposed route:

- LBC Ref: 19/01025/FUL: Refurbishment including external alterations to existing building together with a change of use to mixed use residential C3 (58 units) and community use D1 (750 sqm) plus associated works including minor demolition, landscaping and parking.

3.13 The following planning decisions are relevant to the application. It relates to the whole masterplan area. The detailed elements of the planning permission has been implemented, with extensions and alterations to the Fairfield Halls undertaken together with demolition of the multi-storey car park and parts of the College Green podium.

- LBC Ref 16/00944/P: Outline planning permission for demolition and redevelopment to provide: flexible class A1 (shops) and/or class A2 (financial and professional services) and/or class A3 (food and drink); class B1 (business); class C1 (hotel); class C3 (dwelling houses); class D1 (non-residential institutions); class D2 (assembly or leisure); public realm and landscaping; and associated car and cycle parking, servicing, and access arrangements (with all matters reserved); and

Full planning permission for demolition including multi-storey car park and Barclay Road Annexe; extensions and alterations to Fairfield Halls including class A3 (food and drink); erection of buildings for flexible class A1 (shops) and/or class A2 (financial and professional services) and/or class A3 (food and drink) and/or class D1 (non-residential institutions) and/or class D2 (assembly and leisure) and class C3 (dwelling houses); change of use of basement car park (part) to class D1 (non-residential institutions); public realm and landscaping; and associated car and cycle parking, servicing, and access arrangements. Please note that this permission included comprehensive parameter plans, design guidelines and design codes to guide the development as it came forward over several phases. The site is located in close proximity to a number of recent development sites.

3.14 An application has been granted for alterations to the Fairfield Halls servicing yard:

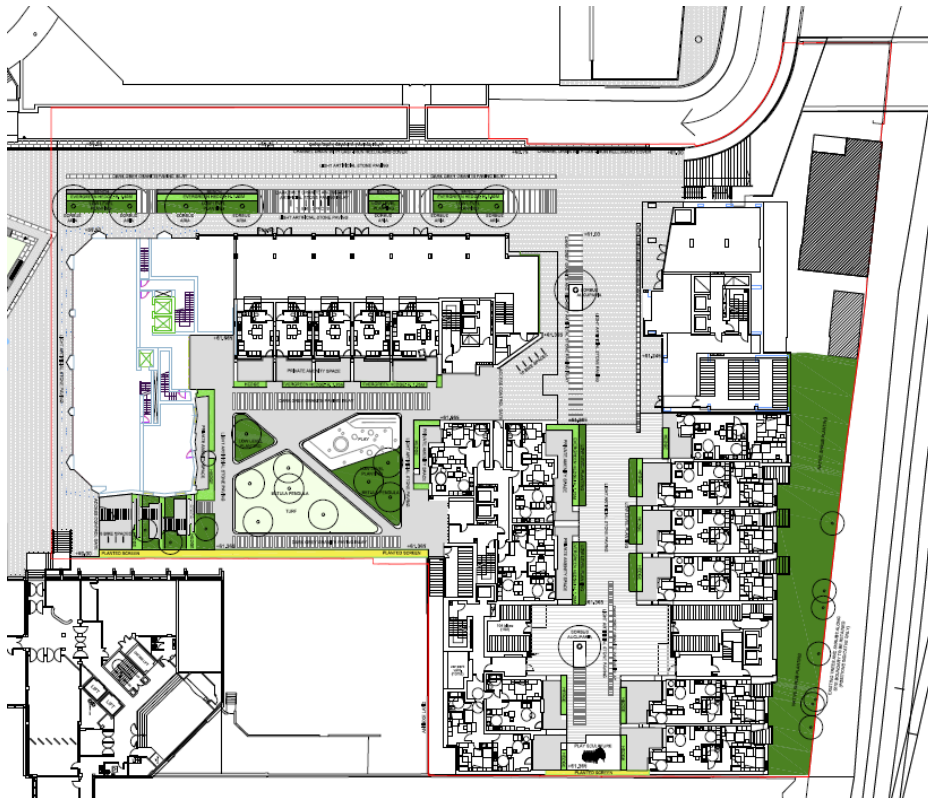
- LBC Ref 19/01249/FUL: Erection of a new loading dock to rear of building and re-configured lower ground level service yard and installation of new plant to the roof of Fairfield Halls

3.15 An application is currently pending consideration for the land to the north and east of the site:

- LBC Ref 19/04516/FUL: Erection of five buildings ranging in height from 7 to 29 storeys to provide 421 residential flats (Use Class C3), flexible commercial space at ground floor of Building A (Use Class A1/A2/A3) and Buildings C and E (A1/A2/A3 and/or B1/D1 or D2) together with associated cycle parking, public realm and landscaping, basement car parking, refuse storage, servicing and access arrangements.



Illustrative render of the proposed scheme (19/04516/FUL)



Site plan for the (19/04516/FUL) proposed scheme (showing relationship with the College Annexe Site to the west and south)

4 PROPOSAL

- 4.1 The proposals seek demolition of the existing building and the erection of a part 4 storey, part 12 storey building providing 90 flats, a public route linking College

Green with Barclay Road and 'commercial space' within part of the ground and first floor for use as a gallery and art studios (in association with the sites allocation for a cultural and creative industries enterprise centre). Amenity space would be provided at ground floor level (the south eastern part of the site). Vehicle access is currently proposed from the Barclay Road ramp.



Proposed ground floor site layout



View from College Green (Fair Field)



View of the ground floor and public route (north elevations)

5 PRP RESPONSE

- 5.1 An earlier iteration of the scheme was presented to the Council's Place Review Panel in September 2019. The scheme presented was for a 10 storey linear block along the northern site boundary. This scheme proposed an enterprise centre across to the entire footprint of the ground floor, with residential above accessed from the north of the site.



Image of the earlier PRP scheme from College Green

- 5.2 The Panel supported some aspects of the proposal but had concerns with the massing, site layout, resolution of the architectural design and public realm. The panel recommended some fundamental design. The Panel's main comments are expanded below:

- 30% single aspect north facing dwellings highlights a fundamental flaw in the organisation
- One panel member suggested that the building could go higher to allow for a more compact footprint in order to mitigate some of the design issues (the Panel had various opinions about the impact of increasing the height and impact upon Fairfield Halls, but suggested that a few options should be considered).
- Further consideration is encouraged to address the articulation of back and front elevations, way finding and access cores.
- The form was considered confusing with ground floor and flank walls not following the same approach as the upper floors. The flank elevations are unresolved. The treatment is critical and should undergo further development.
- The Panel recommended a clear singular sculptural form to the design given the history of the site, using a more confident smaller material palette. With consideration to the pure and applied arts within the architectural surface and working with the sculptural qualities of the proposed materials.
- Involvement from artists in the creation of the surface treatments is encouraged, and this should support the production of public art that is ambitious and contextual.
- The Panel questioned the usability of the balcony spaces
- The panel commended early discussions with an artist workshop provider, but highlighted that the site allocation denotes a more expansive and different type of space potentially including a more collective space where cultural training and different types of cultural production can be supported.
- The Panel felt that artist studios would require more privacy than currently proposed (full length glazing was unlikely to be suitable for studios or gallery space).
- The Panel stressed the importance of the design and business plan, consideration of flexibility for alternative providers in the future and adequate servicing options.
- The yard space to the south of the studios could be used as an interesting space for artistic production, but the shape and layout and any segregation from the public realm needs to be explored.
- The Panel recommended that the space to the south of the site should support bio-diversity/green space as opposed to a high programmed meanwhile use (like Boxpark) and they recommended that a landscape architect be involved as soon as possible to explore the public realm more clearly and understand how it will work including experience of a pedestrian journey along the ramp and boundary between it and the vehicular ramp and the magistrates court.
- The Panel recommended that they explore how ideas from the College Green public realm scheme could be incorporated into this development.
- The play and family living provision is not clear from the current designs and should be more explicit.
- The relationship between the site and adjoining sites is critical to the success of the wider masterplan and the Panel recommended workshops with all stakeholders to address complexities presented by the levels, adjacencies and overlooking issues and how they can work together with the public realm.

5.3 Officers generally agreed with and welcome the comments raised by the Panel. Since the PRP, officers have been working with the applicant, reviewing the footprint of the building and overall design to respond to these comments and the current scheme has been developed as such. The main changes are:

- Height: The massing, scale and layout of the proposed development has been updated to pull in from the boundaries, to increase the height to the western end and reduce the height to the eastern side (to respond to the emerging scheme on the adjacent site which is subject of a current planning application)
- Orientation: The massing of the taller element of the scheme has been re-aligned to improve the orientation of the proposed flats. This has reduced the number of north facing single aspect units.
- Design: The overall design has been rationalised and simplified, including a simple material palette and articulation of the facades. Some initial consideration has been given to potential use of applied arts within the architectural surfaces. Further consideration has been given to the flank elevations (particularly the western elevation which would be visible from College Green and in the context of Fairfield Halls). Further consideration has been afforded to the design of ground floor elevations.
- Layout: The changes to the layout designs improves legibility of the cores, flat arrangements and balconies as well as the articulation of the front and rear elevations. The public realm has been given more consideration (including use of the space to the south of the eastern part of the site for play-space and amenity space). Officers feel that further design development is required for the public route (particularly the open part of the route to the southern spine) but the applicant has started to consider how the public realm design could incorporate the emerging designs from the Fair Field (College Green) public realm proposals.
- Enterprise Centre: The design and layout of this space has been updated to provide a better relationship with the surrounding public realm and the current proposed courtyard (proposed as part of the current scheme to the north and east of the Annexe site) and to consider the usability of the space. Officers have requested the applicant considers an adaptable design for this space should the specific end user not be able to take up the space or should the space be needed by other cultural uses in the future.

5.4 The Panel felt that the relationships between the site and adjoining sites was critical to the success of the wider masterplan. The applicant has considered the adjoining development sites as part of their emerging scheme and has had discussions with adjoining stakeholders to consider how their site could interact with the current proposals for the adjoining site and the emerging public realm scheme. Officers are encouraging and seeking to facilitate further conversations between the various parties.

6 MATERIAL PLANNING CONSIDERATIONS

6.1 The main planning considerations are:

1. Principle of development (land use, affordable housing and open space)
2. Townscape and visual impact

3. Housing Quality for future occupiers
4. Impact upon Neighbours
5. Transport
6. Environmental Impact, Sustainability and Flooding
7. S106 obligations

Principle of Development

- 6.2 The proposed development is a residential led mixed use development with a 'Cultural and Creative Industries Enterprise Centre' and public route through the site connecting College Green (Fair Field) to Barclay Road.

Residential

- 6.3 Residential use of the site is included within the site allocation. The development would contribute to the Borough's strategic housing targets and the overall town centre growth agenda, OAPF and Fair Field Masterplan objectives.
- 6.4 The Council seeks to secure the provision of family sized housing and has an aspiration for 20% of all homes within the Fair Field area to have 3 or more bedrooms. The Local Plan accepts that for the first 3 years of the plan an element of family housing provision may be provided as 2b-4p flats. Overall, 32% of units currently would be family sized units including nine 3 bed flats.
- 6.5 The applicant has confirmed that the current affordable housing offer would be 30% with a 60:40 tenure split (affordable/social rent to intermediate units). This would be subject to viability review; officers are yet to review any viability information as discussions have mostly focused on design and layout.

Community use and Cultural and Creative Industries Enterprise Centre

- 6.6 The uses set out in the site allocation include community use and a cultural and creative industries enterprise centre. Whilst no replacement community space is provided, the proposals do currently include about 750sqm of 'commercial space'. This space is intended by the applicant to provide a Cultural and Creative Industries Enterprise Centre (enterprise centre) in line with the site allocation.
- 6.7 The inclusion of community use reflects the existing use of the site. The existing building is a community facility which is protected Policy SP5 of the Local Plan. The proposals do not include re-provision of the existing community use, which was most recently the Croydon Art School. The Art School has been consolidated within the main Croydon College building since 2012 and the building has been vacant since this date. The building itself is in a poor state of repair. Given the relocation of the existing use and state of the building, it is anticipated that the applicant will seek to justify the loss of the existing use without re-provision. This will allow the applicant to provide a more meaningful space for the proposed enterprise centre.
- 6.8 The Local Plan sets out the Council's aims to support and promote cultural and enterprise growth and expansion in the borough. The site has been specifically allocated as one of 6 sites in the borough to create a network of hubs and

specifically to provide a hub for creative and cultural industries in the CMC. The site allocation states that the site is well suited to provide a home to the enterprise centre for the CMC. The policy notes that the enterprise centre in CMC should act as a flagship for the sector; developing a central location as a hub providing a clear signal that Croydon is 'open for creative businesses'. These spaces are defined in the Local Plan as facilities which promote the start-up and development of small and medium sized businesses in the area through the provision of business information and support services with a focus on creative and cultural industries.

- 6.9 The design of the space currently sets out space for art studios and an art gallery and café space. The design has been influenced by conversations between the applicant, the Council's Cultural Team and Artist Studio Company. Artist Studio Company currently provide low cost art studios across London including locally at the Grafton Quarter (near West Croydon Station). The current design of this space for artist studios would reference the historic use of the site whilst meeting a need for artist studios in the borough and complementing the cultural offer at Fairfield Halls. The general approach is supported by officers. However, officers have also asked the applicant to ensure that flexibility is built into the design of the space and to consider how the space could be used in the future should the demand for cultural space change.

Townscape and Visual Impact

- 6.10 The proposed development requires a high quality architectural approach and public realm to ensure it positively contributes and enhances its significant civic location. The current design is a significant improvement to previous iterations of the scheme, but officers are seeking further design changes to ensure the scheme successfully delivers a high quality scheme.

Massing

- 6.11 Much of the pre-application discussion with the applicant has centred around appropriate heights. The Fair Field Masterplan seeks to ensure that tall buildings are clustered to the north of the Masterplan area, with building heights stepping down in height moving south from East Croydon station towards the heritage assets of Fairfield Halls and Chatsworth Road Conservation Area. In addition, given the location to the south of College Green, a tall building on this site has potential to cause significant overshadowing and micro-climate impacts to College Green which would harm the quality and usability of this important public realm. A previous iteration for a tall building (30 storeys) was not supported by officers for this reason.
- 6.12 The applicant has therefore taken the existing building height as a starting point and sought to respond to the approved height for the eastern part of the site (which has extant permission for a 4 storey block as part of the hybrid permission).
- 6.13 Officers are generally satisfied that the 12 storey element would be suitable, given the surrounding heights approved (and proposed); but further view studies and detailed information as regards materials is required to ensure that the

building would present an acceptable relationship with Fairfield Halls (which is locally listed) when viewed from Park Lane and across College Green. The changes to the west elevation since PRP, with balconies introducing a strong façade design may help the scheme appear distinct should it become visible above Fairfield Halls in some views which may also help minimise any heritage impacts and views of the locally listed building.

- 6.14 Some initial overshadowing analysis has been undertaken by the applicant which suggests that the proposed heights would not result in significant impacts upon College Green. The applicant has modelled the impact of the proposal upon the residential courtyard proposed within the current application for the Fairfield Homes site to the north (which is pending consideration but will be a material consideration when an application is submitted). In the hybrid permission, a similar courtyard was proposed which was intended to be bound by development on all four sides with the lowest heights to the southern side, as would be expected, to gain most sunlight to this space. The initial overshadowing tests suggest that despite the increased heights to the western portion of the site and within the current scheme for the adjoining site, the courtyard should achieve similar sunlight levels as were expected under the hybrid planning permission. Further daylight/sunlight analysis and micro-climate (wind) analysis will be required going forward to ensure that the development will have an acceptable impact.
- 6.15 Whilst the main driver for the heights on this scheme has been the relationship with the proposals to the north and the heritage/townscape implications of height on the site, the move from a singular height across the whole site to a staggered height has allowed the taller element to be angled, providing not only interest to the architectural form, but also improvements to the internal layouts and outlooks for the proposed flats. Officers consider that the move is generally successful, subject to further consideration of the detailed design.
- 6.16 Officers do have some concerns about the potential roof top canopies. The applicant has included these to minimise the design impact of lift overruns providing residents access to the rooftop amenity spaces. These could provide an interesting architectural feature to the building by referencing local mid-century precedents such as Corinthian House. However, the design needs to be further resolved to allay officers concerns that they may result in increased overshadowing.

Elevational Treatment, Fenestration & Materials

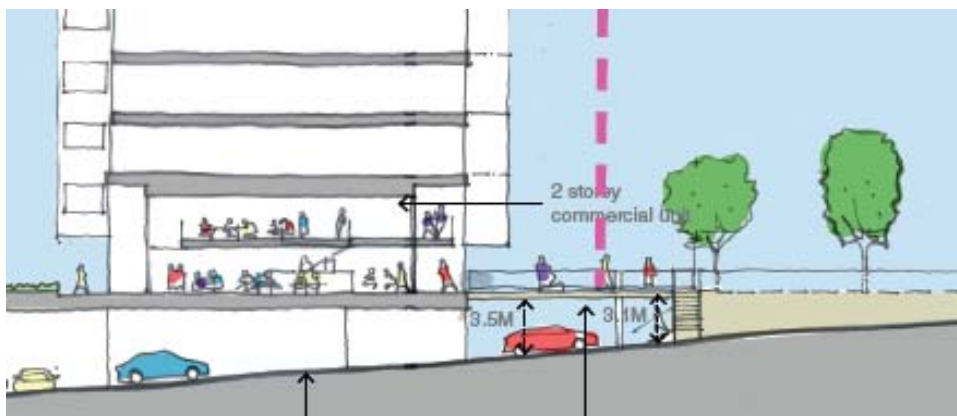
- 6.17 Officers previously requested that the applicant undertakes a comprehensive character appraisal of the area to guide further design development. Initial design development was based upon the angular saw tooth design of the existing building and acoustic panels within Fairfield Halls. Wider consideration of buildings within the CMC has focused upon strong examples of mid-20th Century architecture which is considered appropriate given the relationship with Fairfield Halls and the existing building.
- 6.18 The elevational treatment and materials have been simplified since the PRP feedback. The taller element is proposed to make use of pre-cast concrete

panels with texture to reference the ribbed concrete panels of the existing building. The lower element would make use of a light coloured brick which would provide some differentiation to this element and give it a softer appearance which is generally supported given its more modest scale.

- 6.19 PRP suggested that consideration should be given to use of pure and applied arts within the architectural surface, working with the sculptural qualities of the proposed materials and use of public art. This is particularly relevant given the historic use of the site. The applicant has begun to consider how this could be incorporated into the design of the building. Officers feel that there is opportunity to introduce public art into the architectural surface of the concrete and within the public route through the site particularly within the soffit of the under-croft.
- 6.20 The applicant has sought to give the enterprise centre at ground and first floor a different character to the rest of the building, with the idea that it would sit nestled under the main frame of the building. The form and façade design of this space is under review but the applicant has, in the most recent iteration, simplified the design of this space to provide more of a relationship between it and the rest of the building which also improves the quantum of internal space for the enterprise centre.

Public Realm and Amenity Spaces

- 6.21 Officers consider the site to have significant civic importance and potential which should be embraced in the proposed public realm. The public route through the site to join College Green and Barclay Road is of significant importance to improve the connectivity of the Masterplan Area and beyond, providing a key link from the area to Park Hill Park.



Sketch section showing the level pedestrian route (which would straddle the vehicle access and cut through the building at the same level as College Green)

- 6.22 Officers have been working with the applicant throughout the pre-application process to ensure that this scheme provides a high quality public link - with step free access from College Green through the site and towards Barclay Road. The land level changes make this challenging as does the constrained shape and size. The hybrid permission accepted the principle of a partial under-croft to the

pedestrian route (albeit of shorter length and over a wider route). The width of the pedestrian route is constrained by the need to retain the full width of the Barclay Road ramp for servicing requirements for Fairfield Halls. The intended narrowing of the ramp and reconfiguration of the Fairfield Halls service yard envisaged under the hybrid permission was based upon the wider opportunities presented by the potential for a new college building in this location which would have straddled a wide open pedestrian route.

- 6.23 The Applicant has worked hard to deliver a level route of sufficient width to ensure that it would provide a legible and safe pedestrian route. Further to the PRP comments, the applicant has reviewed the positioning of the route, the use of the southern spine and the edge conditions (against the Magistrates Court boundary and Barclay Road ramp). Since details of the Fair Field public realm design were published in September, the applicant has also begun to consider how their scheme could connect into the wider aims and design parameters of these wider public realm improvements. This is to be encouraged. Officers are seeking to set up workshops to enable engagement and co-ordination between the applicant and public realm design team and to enable discussions with other stakeholders and developers in the Fair Field masterplan area so that this can be explored further as the public realm designs emerge.
- 6.24 An important connection which needs to be resolved is the junction between the northern boundary of the site and College Green. The original building had a gap between the northern elevation and the podium ground level of College Green (of about 1.5m) which was bridged via a set of stairs (due to level differences). Officers feel that the connection between the site and College Green should be seamless, particularly for the pedestrian route, but wider seamless levels and public realm between the western part of the building and College Green would help to activate the ground floor gallery/café and art studio space allowing it to open directly onto College Green (as shown on the plans). This may require some public realm work outside of the Applicant's current redline boundary, but officers feel that this has significant benefits and should be pursued.
- 6.25 Officers feel that the under-croft walkway requires some further design resolution, particularly between the built form and structural columns. The positions for these structural columns are constrained, as this part of the building straddles the public car park access and they will need to line up with the existing vehicle lanes to result in minimum disruption to this existing use. Officers have encouraged a chamfered building line at ground floor to provide more legibility of the route from views across College Green, but further consideration is required of the structural columns to ensure that the chamfer design achieves this legibility.
- 6.26 The applicant has considered some alternatives for the public realm to the southern spine of the site, including potential for pop up uses (such as use of containers for pop up music studios). In officer's view, which to a certain extent aligns with the views expressed by PRP, it would be most appropriate to landscape this space to provide a linear pocket part or similar as a way of connecting College Green to the softer landscape of Park Hill Park. There are some concerns that pop up uses in this location would not provide suitable activation of this space and could result in concerns with regards to legibility and

safety of this route, particularly if structures reduce sight lines along the route. The applicant is currently open to suggestions for this portion of the site and further detailed design development is required. It is worth noting that whilst the applicant is committed to providing a public route through the site, they may seek to develop a further residential block (with pedestrian route within an under-croft arrangement or similar) in the future. Any new application would have to be considered on its merits.

- 6.27 To the south of the proposed building, the podium level has been continued to cover the basement level car park and provide additional amenity space. This would have the added benefit of improving the residential entrance to the second core and providing an area of child play-space for the whole building (although it is noted that additional space may be required at roof level to meet the Local Plan standards). Earlier iterations considered a partial deck in this area (to allow some ventilation to the basement) but the recent iterations show the entire area decked which has the benefit to reducing the visibility of the car parking below and providing amenity space and potential for landscaping. The ground floor flats would also have private amenity spaces in this area. The relationship between public and private circulation space needs further consideration but the recent designs are encouraging.
- 6.28 This is also true of the circulation space to the north of the building. The applicant is seeking to ensure that the levels within the development relate to the proposed College Green/Fairfield Homes podium level and has given some consideration as to how the scheme might connect to the proposed courtyard within the adjacent site to the north which is currently proposed at a slightly raised level (to provide required floor to ceiling heights within the basement for servicing vehicles). Officers have been keen for the applicant to explore how the scheme could connect into these proposed land levels to secure the potential for occupiers from both schemes to make use of the amenity value of the courtyard and access routes (for legibility and quality of accommodation purposes) should an agreement be reached between the two parties (or management companies in the future should both schemes be consented and built-out). The applicant is currently considering the use of steps and a ramp to allow access between the two sites. Officers are supportive of this principle and the use of the College Green land levels (as the more important of the two levels for the site) should ensure that the scheme works should the adjacent site not come forward for any reason.

Access

- 6.29 The proposed land levels and provision of additional public realm/amenity space to the south of the site has opened up the opportunities for residential access points both to the north and south of the building. Although the residential entrances would be accessed through under-crofts from the building above, the general layout is supported.
- 6.30 The positioning of one of the residential entrances on the corner of the public under-croft would help activate this space and provide natural surveillance. The enterprise centre would also have active frontages onto the under-croft space

including to the eastern side, a reception space accessed from the northern corner, and to the western side, a gallery and café.

- 6.31 The articulation of the different uses and entrances within the site have been significantly improved since PRP. Further definition is required between the public and private spaces and how these boundaries would be defined, both in legibility terms and security. The layout of the entrances is considered more rational and whilst the initial sketches and layouts suggest that these will be more legible, officers expect further refinement of the design as the detailed design comes forward.

Housing Quality for Future Occupiers

- 6.32 The flats would all be designed to meet National Technical Space Standards and are all being designed with outdoor amenity space in the form of private balconies or terraces. 10% of the units would need to be designed as wheelchair accessible and the remaining 90% wheelchair adaptable in line with the London Plan.
- 6.33 In addition, the scheme would provide a private area to the south of the site. If this is not sufficient to meet play-space requirements, additional amenity space is proposed to the two roofs of the building which could provide additional child play-space.
- 6.34 Officers and PRP raised some concerns about the number of single aspect north facing units. This has been revised in the latest iterations of the scheme which results in roughly 15% of single aspect north facing units (14 of 90 flats). Of these, the majority have been designed to maximise outlook, making use of angled elevations, providing some outlook to the north-east and north-west. It is acknowledged that the layout of the development site is extremely constrained by its awkward shape and the surrounding context and officers are satisfied that the applicant is working hard to optimise the outlook and layout. The percentage of single aspect north facing units is considered generally acceptable, subject to the receipt of suitable daylight and sunlight to be assessed as part of any application.

Impact upon Neighbours

- 6.35 The adjoining neighbours are currently all commercial buildings, but there is a hybrid planning permission in place and a planning application has been received for a mixed use scheme to the north of the site (Fairfield Homes site). Assuming that this pre application process leads to a planning application submission, both schemes would need to consider and assess reciprocal effects.

Fairfield Halls, servicing and public car park

- 6.36 Fairfield Halls (and service yard), the Magistrates Court and the public car park below College Green (which is open 24/7) are all located in extremely close proximity to the site and none have existing time and noise restrictions. Fairfield Halls in particular is of strategic importance as a cultural venue for the borough. In line with NPPF (and emerging London Plan policies) which seeks to protect existing businesses and facilities from unreasonable restrictions due to new

development, suitable mitigation will be required against noise and other potential nuisances from the existing land uses.

- 6.37 In addition, officers have some concerns that the proposed servicing plans could give rise to conflict between the development and existing uses. These are detailed in the transport section below.
- 6.38 It should be noted that the Magistrates Court formed part of the outline consent for the hybrid permission and it is considered to have some redevelopment potential. It also currently has windows in the rear elevation. The proposed development is considered to be located sufficiently away from the southern boundary of the site to ensure that it would be unlikely to prejudice a future redevelopment of this site nor result in privacy issues for the proposed flats (or result in a poor relationship with the existing windows). The current layout allows the scheme to maximise the landscaping potential and access arrangements to the south of the building.

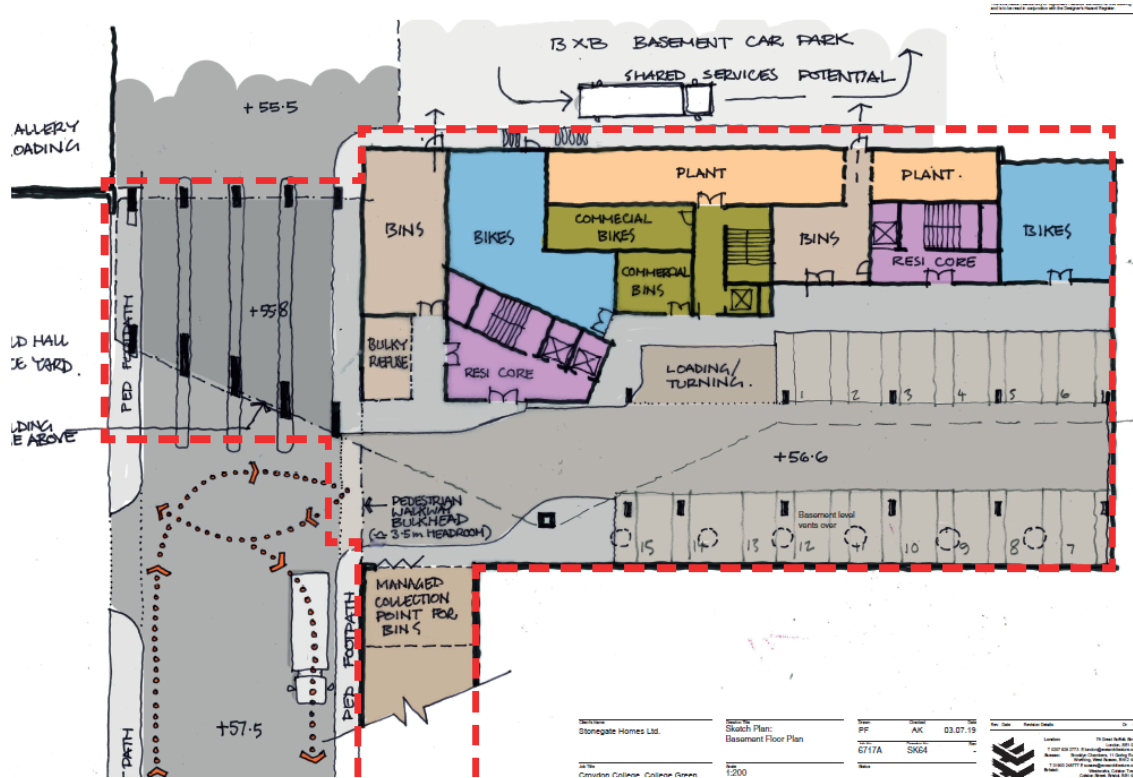
Fairfield Homes site and Fair Field Public Realm scheme

- 6.39 The Fairfield Homes site forms both part of the existing hybrid planning permission and is subject to a new planning application which is currently being assessed. Whilst the extant permission cannot be built, it did set parameters including a residential courtyard to the north of the site which are material considerations. The hybrid expected the courtyard to be bounded on all four sides, with the lowest heights (at 4 storeys) to the southern side of the courtyard. A daylight and sunlight analysis will be required and will need to take into consideration the relationship of both the approved and proposed development on the Fairfield Homes site.
- 6.40 The applicant has sought to design the scheme to meet the proposed 'ground' levels for College Green (Fair Field) as were approved under the hybrid permission but are also considering how the levels could tie into the elevated levels within the Fairfield Homes courtyard. Officers are facilitating discussions with design team for the Fair Field public realm scheme to ensure a coordinated approach with close integration with the wider public realm approach as well as encouraging discussions between adjoining developers to improve the wider coordination with the adjacent site.

Transport

- 6.41 The proposal seeks to provide a car free development with disabled parking provided at basement level accessed from the Barclay Road ramp. The basement access would be limited in height due to the pedestrian route which would straddle the access at a height of about 3.5m (see sketch section at paragraph 6.21 above).
- 6.42 Currently 15 parking spaces are proposed within the basement car park, together with a loading bay / turning head. Each residential core would have lift and stair access to basement level where secure cycle parking and bin storage is proposed. The enterprise centre would have a separate core access to the basement and separate bin and cycle storage. Large servicing vehicles and

waste and recycling vehicles would have to service the site from Barclay Road ramp (turning on the ramp and waiting in the eastern-most lane).



Proposed basement level

- 6.43 The basement layout requires further consideration in terms of vehicle access and movement. As the site is located within an area with high public transport accessibility (PTAL rating 6b), disabled-only parking is acceptable and it would be expected that car parking permits would be restricted for future residents. The number of parking spaces may need to be reduced to ensure that the numbers do not exceed London Plan standards and this would allow for an improved circulation for the space. The site would provide secure cycle storage at basement level accessed via the main residential cores. This will need to be design to meet London Plan standards.
- 6.44 The proposals for servicing vehicles to turn and wait on Barclay Road ramp raises concern in terms of the potential for vehicular conflict. The ramp provides access to the public car park below College Green (Fair Field) and the Fairfield Halls service yard. The car park when it reopens, will be open 24/7 and the Fairfield Halls service yard is anticipated to cater for large articulated lorries. There is no dedicated servicing bay along the ramp, so using the ramp for turning of large vehicles and servicing within the eastern-most lane may result in conflict with these other uses. This, in turn, could result in highway safety issues and cause congestion back onto Barclay Road. Officers have raised this point with the applicant who advised that the existing site was serviced from the ramp. Officers require further transport evidence before this could be supported. Given the concerns raised, the applicant is exploring options for servicing from the adjacent basement car park (the layout of which is under review as part of the

current planning application for this neighbouring site) which would provide a possible solution to overcoming these concerns.

Environmental Impact, Sustainability & Flooding

- 6.45 The applicant has been made aware of the requirements for passive design, zero carbon development and connection to a communal heating network. Discussions are forthcoming in relation to these and air quality, noise, overheating, surface water drainage, micro climate and lighting impacts. The site is within Flood Zone 1 (Low Risk) and partially within an area where there is potential for surface water flooding, and has one known instance of sewer flooding. The applicant has been advised that a flood risk assessment and drainage strategy would be required to support a planning application.

S106 Obligations

- 6.46 At this stage it is envisaged that planning obligations will be required to mitigate the impacts. Discussions are forthcoming in relation to the heads of terms, but it is anticipated that these would include the following:
- Affordable housing (on site and including early and late stage reviews)
 - Delivery of the public realm
 - Employment and Construction training (contributions and obligations)
 - Air Quality
 - Zero carbon off-set (if required)
 - Car club provision and membership
 - Travel Plan
 - Car parking permit restrictions
 - Transport for London contributions
 - Wind mitigation (if required)
 - TV signal mitigation

7 SPECIFIC FEEDBACK REQUESTED

- 7.1 In view of the above, it is suggested that members focus on the following issues:
- The proposed design in terms of both townscape and heritage impacts
 - The design and legibility of the proposed public pedestrian route from Barclay Road to College Green
 - The design and layout of the proposed cultural and creative industries enterprise centre for use as a space for art studios, art gallery and café space
 - The quality of accommodation, including orientation and layout of flats and amenity space
 - The current affordable housing offer of 30% with a 60:40 tenure split (affordable/social rent to intermediate units)

8 PROCEDURAL NOTE

- 8.1 This is the first presentation of the scheme to the Planning Committee. The proposal is reported to Planning Committee to enable Members to view and

comment on it prior to submission of a formal application. The proposal is not a planning application. Any comments are provisional and subject to full consideration, including public consultation and notification as part of any subsequent application.

- 8.2 A planning application for the proposed development would need to be referred to the Mayor of London under the Mayor of London Order 2008. The Mayor's views have not yet been formally sought.

PLANNING COMMITTEE AGENDA

PART 6: Planning Applications for Decision

1 INTRODUCTION

- 1.1 In this part of the agenda are reports on planning applications for determination by the Planning Committee.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 Any item that is on the agenda because it has been referred by a Ward Member, GLA Member, MP or Resident Association and none of the person(s)/organisation(s) or their representative(s) have registered their attendance at the Town Hall in accordance with the Council's Constitution (paragraph 3.8 of Part 4K – Planning and Planning Sub-Committee Procedure Rules) the item will be reverted to the Director of Planning and Strategic Transport to deal with under delegated powers and not be considered by the committee.
- 1.4 The following information and advice applies to all reports in this part of the agenda.

2 MATERIAL PLANNING CONSIDERATIONS

- 2.1 The Committee is required to consider planning applications against the development plan and other material planning considerations.
- 2.2 The development plan is:
 - the London Plan (consolidated with Alterations since 2011)
 - the Croydon Local Plan (February 2018)
 - the South London Waste Plan (March 2012)
- 2.3 Decisions must be taken in accordance with section 70(2) of the Town and Country Planning Act 1990 and section 38(6) of the Planning and Compulsory Purchase Act 2004. Section 70(2) of the Town and Country Planning Act 1990 requires the Committee to have regard to the provisions of the Development Plan, so far as material to the application; any local finance considerations, so far as material to the application; and any other material considerations. Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the Committee to make its determination in accordance with the Development Plan unless material planning considerations support a different decision being taken. Whilst third party representations are regarded as material planning considerations (assuming that they raise town planning matters) the primary consideration, irrespective of the number of third party representations received, remains the extent to which planning proposals comply with the Development Plan.
- 2.4 Under Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990, in considering whether to grant planning permission for development which affects listed buildings or their settings, the local planning authority must have special regard to the desirability of preserving the building or its setting or any features of architectural or historic interest it possesses.

- 2.5 Under Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990, in considering whether to grant planning permission for development which affects a conservation area, the local planning authority must pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area.
- 2.6 Under Section 197 of the Town and Country Planning Act 1990, in considering whether to grant planning permission for any development, the local planning authority must ensure, whenever it is appropriate, that adequate provision is made, by the imposition of conditions, for the preservation or planting of trees.
- 2.7 In accordance with Article 31 of the Development Management Procedure Order 2010, Members are invited to agree the recommendations set out in the reports, which have been made on the basis of the analysis of the scheme set out in each report. This analysis has been undertaken on the balance of the policies and any other material considerations set out in the individual reports.
- 2.8 Members are reminded that other areas of legislation covers many aspects of the development process and therefore do not need to be considered as part of determining a planning application. The most common examples are:
- **Building Regulations** deal with structural integrity of buildings, the physical performance of buildings in terms of their consumption of energy, means of escape in case of fire, access to buildings by the Fire Brigade to fight fires etc.
 - Works within the highway are controlled by **Highways Legislation**.
 - **Environmental Health** covers a range of issues including public nuisance, food safety, licensing, pollution control etc.
 - Works on or close to the boundary are covered by the **Party Wall Act**.
 - **Covenants and private rights** over land are enforced separately from planning and should not be taken into account.

3 ROLE OF THE COMMITTEE MEMBERS

- 3.1 The role of Members of the Planning Committee is to make planning decisions on applications presented to the Committee openly, impartially, with sound judgement and for sound planning reasons. In doing so Members should have familiarised themselves with Part 5D of the Council's Constitution 'The Planning Code of Good Practice'. Members should also seek to attend relevant training and briefing sessions organised from time to time for Members.
- 3.2 Members are to exercise their responsibilities with regard to the interests of the London Borough of Croydon as a whole rather than with regard to their particular Ward's interest and issues.

4. THE ROLE OF THE CHAIR

- 4.1 The Chair of the Planning Committee is responsible for the good and orderly running of Planning Committee meetings. The Chair aims to ensure, with the assistance of officers where necessary, that the meeting is run in accordance with the provisions set out in the Council's Constitution and particularly Part 4K of the Constitution 'Planning and Planning Sub-Committee Procedure Rules'. The Chair's most visible responsibility is to ensure that the business of the meeting is conducted effectively and efficiently.
- 4.2 The Chair has discretion in the interests of natural justice to vary the public speaking rules where there is good reason to do so and such reasons will be minuted.

- 4.3 The Chair is also charged with ensuring that the general rules of debate are adhered to (e.g. Members should not speak over each other) and that the debate remains centred on relevant planning considerations.
- 4.4 Notwithstanding the fact that the Chair of the Committee has the above responsibilities, it should be noted that the Chair is a full member of the Committee who is able to take part in debates and vote on items in the same way as any other Member of the Committee. This includes the ability to propose or second motions. It also means that the Chair is entitled to express their views in relation to the applications before the Committee in the same way that other Members of the Committee are so entitled and subject to the same rules set out in the Council's constitution and particularly Planning Code of Good Practice.

5. PROVISION OF INFRASTRUCTURE

- 5.1 In accordance with Policy 8.3 of the London Plan (2011) the Mayor of London has introduced a London wide Community Infrastructure Levy (CIL) to fund Crossrail. Similarly, Croydon CIL is now payable. These would be paid on the commencement of the development. Croydon CIL provides an income stream to the Council to fund the provision of the following types of infrastructure:
- i. Education facilities
 - ii. Health care facilities
 - iii. Projects listed in the Connected Croydon Delivery Programme
 - iv. Public open space
 - v. Public sports and leisure
 - vi. Community facilities
- 5.2 Other forms of necessary infrastructure (as defined in the CIL Regulations) and any mitigation of the development that is necessary will be secured through A S106 agreement. Where these are necessary, it will be explained and specified in the agenda reports.

6. FURTHER INFORMATION

- 6.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

7. PUBLIC SPEAKING

- 7.1 The Council's constitution allows for public speaking on these items in accordance with the rules set out in the constitution and the Chair's discretion.

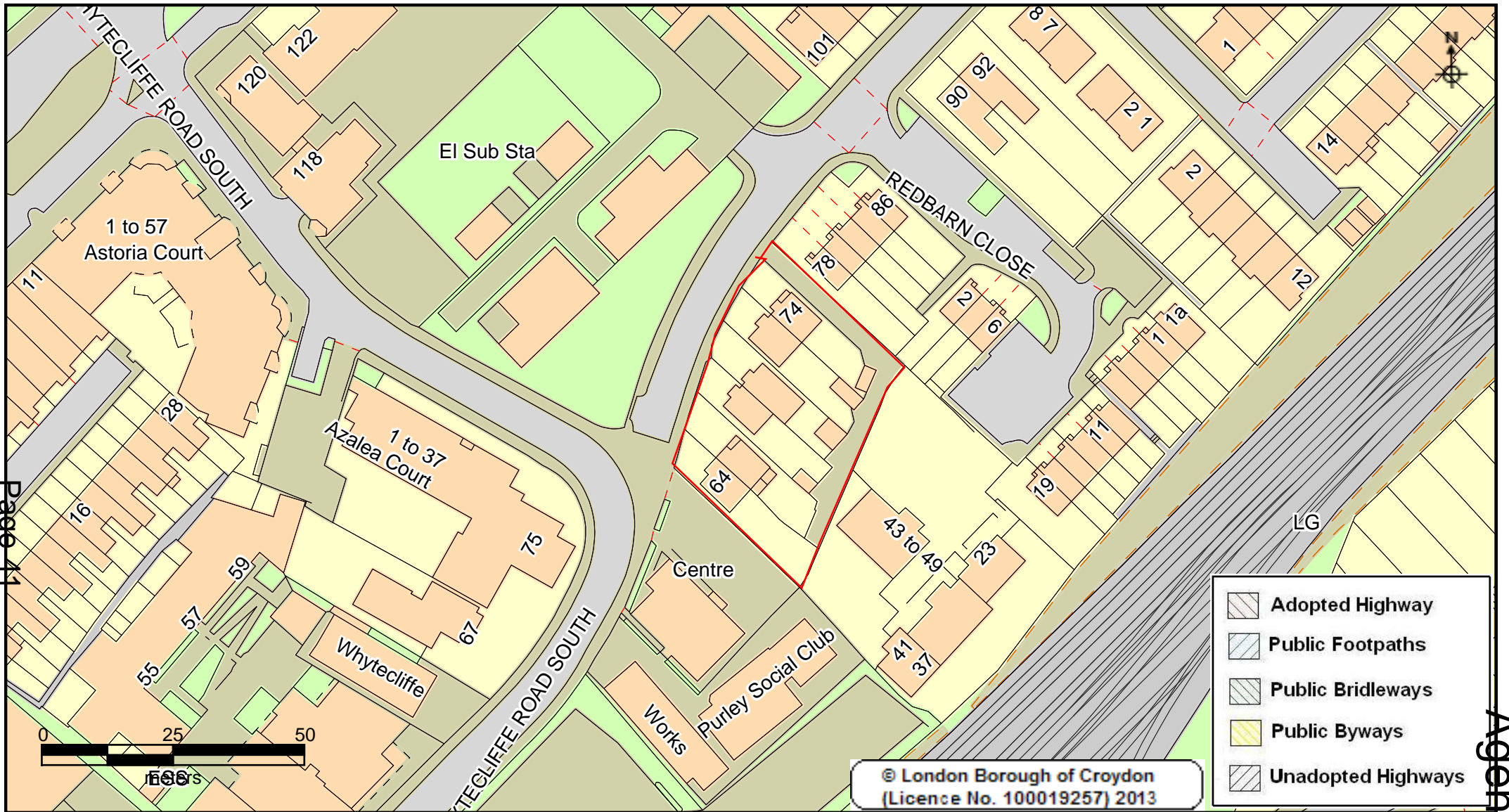
8. BACKGROUND DOCUMENTS

- 8.1 The background papers used in the drafting of the reports in part 6 are generally the planning application file containing the application documents and correspondence associated with the application. Contact Mr P Mills (020 8760 5419) for further information. The submitted planning application documents (but not representations and consultation responses) can be viewed online from the Public Access Planning Register on the Council website at <http://publicaccess.croydon.gov.uk/online-applications>. Click on the link or copy it into an internet browser and go to the page, then enter the planning application number in the search box to access the application.

9. RECOMMENDATION

- 9.1 The Committee to take any decisions recommended in the attached reports.

This page is intentionally left blank



This page is intentionally left blank

PART 6: Planning Applications for Decision

Item 6.1

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/02678/FUL
 Location: 64 to 74 Whytecliffe Road North, Purley, CR8 2AR.
 Ward: Purley Oaks and Riddlesdown.
 Description: Demolition of three pairs of semi-detached houses, erection of a part 3/part 5/part 6 storey building with basement to provide 39 residential units, with associated terraces, disabled car parking spaces, amenity spaces and landscaping.
 Drawing Nos: S0R013678 03, 6665-D8106 rev 2, D8199 rev 03, D8500 rev 02, D8700 rev 02, D8701 rev 02, D8100 rev 07, D8101 rev 03, D8102 rev 01, D8103 rev 02, D8104 rev 01 and D8105 rev 02.
 Applicant: RAA Development Group Ltd and Regent Land and V Fund
 Agent: Mr Kevin Goodwin of KG Creative Consultancy
 Case Officer: Barry Valentine

	Market Housing	Affordable Rent	Intermediate	TOTAL
Studio	0	0	0	0
One-bed	7	2	1	10
Two-bed	17	4	3	24
Three bed	3	1	1	5
TOTAL	27	7	5	39

Number of car parking spaces	Number of cycle parking spaces
2 on site car parking spaces	76 on site cycle parking spaces

1.1 This application is being reported to Planning Committee because objections above the threshold in the Committee Consideration Criteria have been received.

2.0 BACKGROUND

Pre-Application Advice

2.1 An earlier version of the proposal was presented to Planning Committee at pre-application stage on 11th April 2019. It was presented as part of a larger redevelopment that also included Nos. 26 to 52 Whytecliffe Road South (this site is subject to a separate planning application reference 19/03142/FUL, see 5.4 below). A summary of the advice provided is as follows:

- Positive feedback on the proposal from Members who noted the scheme was developing well. Members liked the idea that the sites were to be used for residential, which will help revitalise the local economy.
- Some Members suggested the applicant should explore additional height on sites A (application site), B and C (26 to 52 Whytecliffe Road South) and address the issue of stepping down and whether this was an appropriate design solution, subject to daylight and sunlight.
- Members welcomed 30% affordable housing offer and would welcome more.
- The balconies must be useable and recessed. Glass balconies and screens are not supported by Members.
- The building rhythm is working well with the height and massing. The applicant should explore distinct brickwork and detailing to help with the architecture and rhythm.
- Members are satisfied with the level of parking given the accessible location. The play areas should not be segregated between market and affordable housing and the access to these areas must be well considered.
- Mitigation for the site from railway noise must be designed at an early stage.

Place Review Panel

2.2 The proposal was presented to Place Review Panel on the 21st February 2019. The site was again presented as part of a larger masterplan, which also included nos. 26 to 52 Whytecliffe Road South (19/03142/FUL), and land between the two sites i.e. Network Rail Car Park, the vacant Health Centre, Purley Social Club and Elysuim House. The comments most relevant to this development were as follows:

- The Panel felt the scheme has great potential to make a very positive contribution to the townscape of Purley and provide high quality residential accommodation and supported some of the approaches towards the elevational treatment, in particular the subtle use of brickwork.
- The Panel felt that all efforts should be taken to encourage Network Rail to bring forward its site simultaneously to allow for a comprehensive development of the 'masterplan' area.
- The amenity and play space provision required further development.
- Given its central location, the development requires its own unique character and to avoid overly repetitive elevational treatments which currently give the development a monolithic appearance.
- The height should vary more within the development to provide design interest.
- Prominent corners should be given considerable attention.
- The public realm and landscape design requires substantial development.
- The development for Site A (application site) should be lower in scale than proposed [same height as present although slightly different form] to be more sympathetic to the scale of its context. Its material finishes should also vary from those of the development on Sites B and C (nos. 26 to 52).

2.3 The scheme has been amended since the Committee and PRP sessions and has sought to overcome the issues raised.

3.0 SUMMARY OF KEY REASON FOR RECOMMENDATION

- 3.1 The provision of 39 residential units would make a contribution to housing delivery in a well-connected location. 49% of these units would be 2 bed 4 person or larger, and would positively contribute to family housing provision.
- 3.2 The proposed development would provide 31.5% affordable housing by habitable room, which amounts to 12 units, at a 57 to 43 split between affordable rented homes and intermediate homes. This offer has been independently scrutinised and is the maximum reasonable affordable housing policy compliant provision.
- 3.3 The scale of the development is an appropriate response to the site's location and neighbouring properties, with a high quality design and detailing, with further details on materials recommended to be secured via condition. A high quality landscaping scheme is proposed that compensates for the loss of 2 existing poor quality trees.
- 3.4 The proposed development would not have an unacceptable impact on neighbouring properties' living conditions.
- 3.5 The standard of residential accommodation would be acceptable, as all units would meet the Nationally Described Space Standards (NDSS), would have sufficient private amenity space and access to sufficient communal amenity and child play space. All units would have an acceptable level of access to light and outlook.
- 3.6 To ensure that the development promotes sustainable modes of transport, and to offset any risk of the development exasperating existing parking stress, the applicant has agreed to financial contribution to facilitate the expansion of the Purley Control Parking Zone.
- 3.7 Suitable planning obligations and conditions have been recommended in order to ensure that the proposed development does not have an adverse impact upon either air quality or the risk of flooding.

4.0 RECOMMENDATION

- 4.1 That the Planning Committee resolve to GRANT planning permission subject to:
 - A. The prior completion of the legal agreement to secure the following planning obligations:
 1. Affordable Housing
 2. Local Employment and Training Strategy and Contribution – Construction Phase (£16,960)
 3. Travel Plan
 4. Carbon Offset Financial Contribution (£27,000)
 5. Air Quality Contribution (£3,900)
 6. CPZ Expansion Contribution and Parking Permit Restrictions (£50,000)
 7. Highway Works

8. Car Club Contribution (£12,500)
 9. Pocket Park Contribution (£25,000)
 10. Monitoring Fees
 11. Any other planning obligation(s) considered necessary by the Director of Planning and Strategic Transport.
- 4.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the legal agreement indicated above.
- 4.3 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. In accordance with the approved plans.
2. Development to be implemented within three years.
3. Samples and details (as appropriate) of materials including window frames and balustrades.
4. Secure by Design Principles
5. Details on landscaping including replacement trees, biodiversity mitigation measures, boundary treatments, child playspace and communal areas.
6. Public Art
7. Secure Arboricultural Method Statement
8. Secure Flood Risk Assessment and Basement Impact Assessment.
9. Secure Sustainable Urban Drainage System.
10. Details on District Energy Future Connection Design Measures
11. Construction Logistics Plan (Prior to Commencement)
12. Secure Delivery Service Plan
13. Land Contamination – Site Investigation and Remediation
14. Provision of on-site car parking – prior to occupation and permanently retained thereafter. All to be installed with electric vehicles charging point.
15. Cycle Parking – Further Details and Implementation
16. Refuse to be built and completed prior to occupation.
17. 90% of units to meet M4 (2) accessibility standard.
18. 10% of units to meet M4 (3) accessibility standard.
19. Water use target.
20. Obscurely glazed and non-opening windows up to 1.7m on eastern flank elevation at first and second floor level.
21. Secure Noise Impact Assessment
22. Noise from any plant and machinery
23. Ultra NOx Boiler
24. Energy Efficiency
25. Secure Ecology Report and Surveys
26. Lighting Plan
27. Balcony and Terrace Management Plan
28. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport.

Informatives

1. Community Infrastructure Levy.
2. Subject to legal agreement
3. Thames Water Advice
4. Pollution and Noise from Demolition and Construction Sites Guidance.
5. Refuse Informative
6. Any other informative(s) considered necessary by the Director of Planning and Strategic Transport.

4.4 That the Planning Committee confirms that adequate provision has been made, by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

4.5 That, if by within 3 months of the planning committee meeting date, the legal agreement has not been completed, the Director of Planning and Strategic Transport has delegated authority to refuse planning permission.

5.0 PROPOSAL AND LOCATION DETAILS

Proposal

5.1 Planning permission is sought for demolition of three pairs of semi-detached houses, and subsequent erection of a part 3/part 5/part 6 storey building with basement to provide 39 residential units, with associated terraces, two disabled car parking spaces, amenity space and landscaping.



Image A – CGI of Proposed Scheme

Site and Surroundings

5.2 The application site (0.16 hectares) comprises three pairs of two storey semi-detached houses fronting onto the south eastern side of Whytecliffe Road North. All six properties are single dwellinghouses (C3). Whytecliffe Road North is a cul-de-sac with the road terminating at the south eastern end of the site. At the dead end of the street there is a small public space containing trees and cycle paths that provide connection between Whytecliffe Road North and Whytecliffe Road South. The site slopes by 6m down from north to south.

- 5.3 The site lies at the periphery of Purley Town Centre, which is 100m to the west. To the north east of site are series of two storey suburban residential properties. To the south east of the site is Redbarn Close, which contains two and three storey residential flatted development. To the south west of the site is Whytecliffe Road South, which contains a mixture of building types and land uses, including vacant Purley Community Health Centre (adjoins the site), the entrance to Purley Train Station, two car parks including a multi storey car park, residential properties including flatted developments up to five storeys high and commercial properties including offices and shops.



Image B – Birds Eye View of the Site

- 5.4 The site has a Public Transport Accessibility Level (PTAL) of 5 (very good), is located within Flood Risk Zone 1, as defined by the Environmental Agency and is at Very Low (less than 1 in 1000 years) risk from Surface Water Flooding. The site is in an area where there is potential for groundwater flooding to occur at surface.
- 5.5 The site is not in a conservation area and there are no heritage assets on the site, nor directly adjoining. There are no listed buildings or conservation areas that would be adversely affected by the proposed development. Brighton Road (Purley) Local Heritage Area lies approximately 100m west of the site.
- 5.6 The Network Rail Car Park, nos. 54 to 58 Whytecliffe Road South is an allocated site within the Croydon Local Plan (2018) for 'Residential use for up to 119 homes with retention of car parking spaces'.

Relevant Planning History

- 5.7 The council provided pre-application advice reference (18/02268/PRE) in connection with this site.

26 to 52 Whytecliffe Road South

- 5.8 A planning application reference 19/03142/FUL was made valid on the 8th July 2019 for the following description of development 'Demolition of existing terraced houses and erection of part 6/part 7/part 8/part 9 development to provided 106 residential units, together with five wheelchair parking spaces and landscaping.' The application is still under consideration by officers.

6.0 LOCAL REPRESENTATION

- 6.1 A total of 54 neighbouring properties were notified about the application and invited to comment by the way of letter, two site notices were erected and a notice published in the press. The number of representations received from neighbours, local groups etc. in response to notification and publicity of the application were as follows:

Individual responses: 39, Objections: 37, Support: 12 (1 is conditional support)

- 6.2 The following issues were raised in representations that are material to the determination of the application, which are addressed in substance in the next section of this report:

Summary of Objectors Concerns	Officer's Response
Lack of on-site parking spaces	The low level of parking provision is considered appropriate given the sites close proximity to public transport and its close proximity to the town centre.
Development will cause parking stress	The applicant has agreed to a financial contribution that would be used to implement the expansion of the Purley Controlled Parking Zone. All residents would not have an entitlement to apply for a parking permit, thus ensuring that the development would not have an unacceptable impact on parking stress.
Road not suitable for extra traffic, and could be a safety risk.	The development would have a minimal impact on traffic generation and is not considered to pose a safety risk.
Development not in keeping with character of Whytecliffe Road North, Purley and the Local Heritage Area.	The development is considered to be of an appropriate scale and form, and is a high quality design that would not cause harm to the character of Whytecliffe Road North, Purley or the Local Heritage Area (which is over 100m away).
Opposition to the creation of flats.	Flats form a valuable source of housing and are not opposed in principle.
Loss of privacy to neighbouring gardens.	The development, subject to conditions, would not cause an unacceptable loss of neighbouring privacy.

Loss of light.	The development would not cause unacceptable loss of light to neighbouring properties.
Development will cause crime.	The creation of flats is not considered to cause crime. A condition is also recommended in regards to secure by design, to ensure that crime prevention measure are incorporated into the building.
Negative impact on public transport and local infrastructure.	The impact of the development on public transport and local infrastructure would be mitigated through the collection of CIL.
Due to height level differences the development will be overbearing to neighbouring properties.	The development is of an appropriate height and form and would not be unduly overbearing to neighbouring properties.
People using the balconies and common areas will cause noise disturbance to neighbouring properties.	It is not considered that the use of terraces or communal areas would generate significant levels of noise disturbance given the residential nature of the development and its location.
Concern about how the development will be serviced.	A draft delivery service plan has been provided, the final delivery service plan is recommended to be secured via condition.

6.3 Eleven letters of support have been received stating the following:

- The two sites (application site and 26 to 52 Whytecliffe Road South) will provide desperately needed homes ideal for time buyers and downsizers in a sustainable location, close to public transport links.
- Both applications will provide 30% plus of much needed affordable housing.
- Significant improvements on the landscaping, street scene, pavement and environment on Whytecliffe Road.
- High-quality designed homes which complement the architecture in Purley and will make Whytecliffe Road fantastic for years to come.
- Provision of 3 bed homes, perfect for families.

6.4 Purley and Woodcote Residents Association support the scheme subject to the following being met:

- Condition of development requires external design and finishes to be of high quality with brickwork of varying colours.
- Interest should be added through balconies and landscaping.
- That the development does not preclude the proposed larger integrated development to the south.

7.0 CONSULTATION RESPONSE

7.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

Lead Local Flood Authority (LLFA) (Statutory Consultee)

No objection as the submitted drainage strategy is in line with LLFA requirements. A condition is recommended to secure the submitted drainage strategy.

Greater London Archaeology Advisory Service (GLAAS)

No objection as there is no discernible on-going archaeological potential for the site. No further assessment or conditions deemed necessary.

Thames Water

No objection but have provided comments for the developer that have been added as an informative.

Designing Out Crime Officer

No objection but recommends that a condition be attached in regards to secure by design accreditation.

8.0 RELEVANT PLANNING POLICIES AND GUIDANCE

8.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan and any other material considerations. Details of the relevant policies and guidance notes are attached in Appendix 1.

National Guidance

8.2 The National Planning Policy Framework 2019 (NPPF) and online Planning Practice Guidance (PPG), as well as the National Design Guide (2019) are material considerations which set out the Government's priorities for planning and a presumption in favour of sustainable development.

8.3 The following NPPF key issues are in particular relevant to this case:

- Delivering a sufficient supply of homes
- Ensuring the vitality of town centres
- Promoting healthy and safe communities
- Promoting sustainable transport
- Making effective use of land
- Achieving well-designed places
- Meeting the challenge of climate change, flooding and coastal change

Development Plan

8.4 The Development Plan comprises the London Plan 2016 ("London Plan"), the Croydon Local Plan 2018 ("Local Plan"), and the South London Waste Plan 2012.

8.5 There is a new draft London Plan that has undergone Examination in Public (EiP) and it is expected that the final document will be adopted by spring 2020. The

current 2016 Consolidation Plan is still the adopted Development Plan. However the Draft London Plan is a material consideration in planning decisions and will gain more weight as it moves through the process to adoption. At present the draft plan in general is considered to carry some weight.

8.6 The relevant Development Plan policies are in Appendix 1.

Supplementary Planning Document

8.7 The relevant SPGs and/or SPDs are listed in Appendix 1.

9.0 MATERIAL PLANNING CONSIDERATIONS

9.1 The main planning issues raised by the application that the Planning Committee are required to consider are:

1. Principle of development, affordable housing and quality of residential units
2. Impact on the appearance of the site and surrounding area.
3. Impact of the development on neighbouring properties' living conditions.
4. Impact of the development on parking and the highway.
5. Impact of the development on trees.
6. Impact of the development on flooding.
7. Other planning issues.

Principle of development, affordable housing and quality of residential units.

Principle of Development

- 9.2 The London Plan (2016) sets a minimum ten year target for the borough of 14,348 new homes over the period of 2015-2025. The Croydon Local Plan (2018) sets a minimum twenty year target of 32,890 over the period of 2016 to 2036. The proposed development would create additional residential units that would make a contribution to the borough achieving its housing targets as set out in the London Plan (2016) and Croydon Local Plan (2018). The site has good access to public transport, local shops and services and is therefore well placed for high density residential-led development, and therefore is in principle supported.
- 9.3 Policy SP2.7 sets a strategic target of 30% of all new homes up to 2036 to have three beds or more. The policy sets a specific target for major developments based on the character of the area and PTAL rating. For this site which is an Urban Area with PTAL 5, the target is 40%. The policy does allow for two bed four person units to be provided in lieu of three bed units when within the first three years of the plan and where a viability assessment has demonstrated that larger homes would not be viable.
- 9.4 Five of the proposed units are three bed units which amounts to 13% of the total number of units. Increasing the number of three bed units would reduce viability and prevent the development providing the optimum amount of affordable housing in line with policy requirements. There are 14 two bed four person units

and as such 49% of the units are therefore 2 bed 4 person units or larger. The proposal complies with Policy SP 2.7.

- 9.5 Policy DM1.2 seeks to prevent the loss of small family homes by restricting the net loss of three bed units and the loss of units that have a floor area less than 130 sq.m. All six of the existing properties would have been three beds when originally built and are all less than 130 sq.m in size. The proposed development is therefore strictly contrary to policy DM1.2 with five three bedroom homes being provided. However, balancing this against the provision of 14 two bed four person units, the delivery of 13 affordable units, 6 of which are 2 beds 4 person or larger, and as such suitable to be used by small family homes and the uplift in residential units on site, no objection is raised. There would be a net gain in affordable family housing provision as a result of the development.

Affordable Housing

- 9.6 London Plan (2016) policy 3.9 is clear that communities which are mixed and balanced by tenure and household income should be promoted across London, through incremental small scale as well as larger scale developments, which foster social diversity, readdress social exclusion. In relation to tenure, London Plan policy 3.10 defines affordable housing as social rented, affordable rented and intermediate housing, provided to eligible households whose needs are not met by the market. The need for affordable housing is so acute, the Mayor of London (via London Plan policy 3.11) requires Boroughs to set affordable housing targets.
- 9.7 The Croydon Local Plan (2018) requires the Council to seek a minimum of 30% affordable housing, but negotiate to achieve up to 50% affordable housing (subject to viability), and seek a 60:40 split between affordable rented homes and intermediate homes.
- 9.8 The proposed development would provide 31.5% affordable housing by habitable room, which amounts to 12 units. The tenure splits would be 57% affordable rent to 43% shared ownership by habitable room, which translates to 7 affordable rent units and 5 shared ownership units.
- 9.9 The applicant's affordable housing offer has been independently scrutinised by Savills on behalf of the council. The results of the viability appraisal is that there would be a viability deficit and it would not be viable to provide an increased amount of affordable housing. The proposed level of affordable housing is the maximum reasonable, policy compliant and thus acceptable.

Residential Quality

- 9.10 The proposed development would provide high quality residential units. All the proposed units would meet recommended minimum floorspace standards set out in both the London Plan (2016) and DCLG's 'Technical Housing Standards: National Described Space Standards'. All the bedrooms would meet the minimum floor areas set out in the DCLG's 'Technical Housing Standards: National Described Space Standards'.

- 9.11 All units would receive good levels of daylight by virtue of being dual aspect and as key habitable rooms would be served by generously sized windows. All units would have floor to ceiling heights of 2.7m, which is excess of the 2.5m required under the London Plan (2016).
- 9.12 In regards to private amenity space, policy DM 10.4 (c) requires new residential developments to provide a minimum of 5sq.m per 1 to 2 person units, with an additional 1 sq.m per extra occupant thereafter. All the proposed units would have a private terrace or balcony that would comply with Council's policy standards.
- 9.13 All residential units would have access to a 550 sqm communal garden located to the rear of the property. The communal external amenity space is of high quality incorporating new and existing trees, a diverse range of planting, child's playspace and seating.
- 9.14 Under Policy DM 10.5, the proposed development based on child yield is required to provide 97.3 sq.m of child's playspace. Playspace is provided throughout the rear communal garden area, with formal play equipment including large slide, nest swing, play sculptures and tepees, as well as equipment to encourage less formal play such as play mounds, timber stepping logs and boulders. There is estimated to be 150 sq.m of playspace, as such the proposal complies with Policy DM 10.5.
- 9.15 In regards to accessibility, London Plan Policy 3.8 'Housing Choice' requires 90% of dwellings to meet M4(2) 'accessible and adaptable dwellings' Building Regulations requirement, with the remaining 10% required to meet M4(3) 'wheelchair user dwellings'.
- 9.16 The applicant has confirmed that four units (AG1, AG4 A14, A24) would be to M4 (3) 'wheelchair user dwellings' standard, whilst all remaining units would be M4 (2) 'accessible and adaptable dwellings'. Two accessible parking bays have been provided.
- 9.17 A Noise Impact Assessment has been submitted with the application which demonstrates that through consideration of the building envelope design, future residents would not be exposed to harmful levels of noise. The mitigation proposed in the Noise Impact Assessment is recommended to be secured via condition.

Impact on the Appearance of the Site and Surrounding Area.

- 9.18 The existing properties are not protected from demolition. As such, they could be demolished under existing permitted development rights through the prior approval process without planning permission. The demolition of the existing building is acceptable subject to conditions.
- 9.19 The development is made up of two main components; a smaller three storey element and a five storey plus roof storey main element. This approach allows

the development to successfully balance its role to the periphery of the town centre, between the suburban two to three storey properties to the north east and the more varied but generally taller building heights that surround Purley Town Centre, where properties extend up to seven storeys in height. The site's corner location provides further flexibility which also helps support the proposed massing of the development. The modest height of the development would ensure that it would not have an adverse impact on Brighton Road (Purley) Local Heritage.



Image C (left) – CGI of Development from substation. Image D (right) – CGI of Development from Whytecliffe Road South looking north.

9.20 The three storey element appropriately aligns with the front building line of the adjoining terraces nos. 78 to 86 Whytecliffe Road North. This ensures that the development would not be dominant in views along Whytecliffe Road North. The front building line appropriately staggers back to respond to the curvature of the road. These series of staggers and recess are not only a natural response to the road, but also help break up the massing in views from Whytecliffe Road South. The rear building line is set at an appropriate depth, with the development not intercepting a 45 degree line on plan as measured from windows located on the rear elevation of no. 78. A generous 5.5m gap would be maintained between the flank elevation of the development and the flank elevation of no.78.



Image E – Proposed Ground Floor Plan

- 9.21 The development has been drawn up by the applicant as part of a wider masterplan to ensure that the bulk, massing and general design approach is well considered and does not compromise the delivery of the adjoining allocated site, as well as potential future sites, including 26 to 52 Whytecliffe Road South (subject to a separate planning application reference 19/03142/FUL).
- 9.22 The primary material would be London Stock Brick, which is a high quality robust material choice but which also helps embed the development into its existing context. The development features two different tones of brick, a dark red and a lighter red. The two colours whilst having an inter-relationship, help break up the mass and define differing elements. Brick banding helps define the levels within the building, adding interest and providing welcomed horizontality to the design. The materiality and detailing of the recessed top floor helps express it as a roof form and ensures that it acts as appropriate termination point. Windows frames, door frames and balustrades will have a brown/brass tone that has a relationship with the tone of the rest of the building. Most of the balconies would have brick balustrade, except four which will be metal. The applicant has agreed to a condition requiring the submission of a balcony management plan to help ensure that bulky items are not stored on balconies and that reed/bamboo screens are not installed which would detract from the appearance.



Image F – Coloured Elevation of Proposed Development



Image G – CGI of roof form detailing

9.23 The spaces adjacent to the main entrance of the development and at roof level (indicated with red dash in image F above) have been designated for the display of public art. The developer will advertise a competition for local artist, with general/local public involved in the selection process. Part of the brief will be for the public art to relate to local activity or local heritage. The public art will add visual interest to the building, will help to embed the development into its local context and help reinforce local distinctiveness.

Impact on Neighbouring Properties Living Conditions

9.24 The applicant has submitted a sunlight and daylight study that tests the scheme against guidance contained with BRE's 'Site Layout Planning for Daylight and Sunlight - A Guide to Good Practice' End Edition, 2011'. See Appendix 2 for BRE sunlight and daylight definitions. The assessment measures the impact of the development on the following properties; Azalea Court, 78 Whytecliffe Road North and 43 to 49 Redbarn Close.

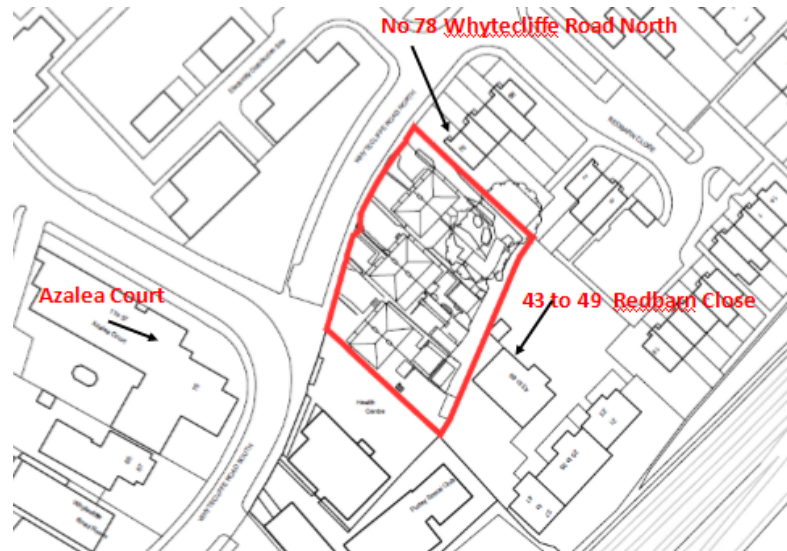


Image G – Location of Properties tested

9.25 In terms of VSC, all windows located on the principal elevations of the existing neighbouring dwellings will fully comply with BRE guidelines. Three secondary windows do fall below BRE guideline standards. The three windows that fail are shown in the images below, with the two windows on the image to the left belonging to 43 to 49 Redbarn Close and the window at ground floor in the picture on the right belonging to 78 Whytecliffe Road North. The two windows on Redbarn Close given their position and type (i.e. high level and located on the flank elevation) are likely to serve non habitable rooms, and as such the development's impact on them is acceptable. The side window to no.78 Whytecliffe Road North VSC would reduce from 25.7% to 10.12%, which is a 60.62% reduction. This window does serve a habitable room, probably a lounge. The window faces in an unneighbourly direction and has an undue expectation of light over the applicant's land which should not limit the development potential of the site.



Image I – Image and Location of Windows that Fail BRE Guidelines

9.26 In terms of daylight distribution, all windows except those that fail VSC discussed above pass BRE's daylight distribution guidelines. In terms of sunlight all windows except the one located on the side wall of no.78 at ground floor level would pass BRE Sunlight and daylight test. In conclusion, the proposed development would not cause unacceptable harm to neighbouring properties sunlight and daylight.

9.27 The proposed development would not cause unacceptable harm to neighbouring properties' outlook due appropriate front and rear building lines, with the later not subtending an angle of 45 degrees as taken from the rear windows of the neighbouring property or even when taken from the edge of the building as in the image below.

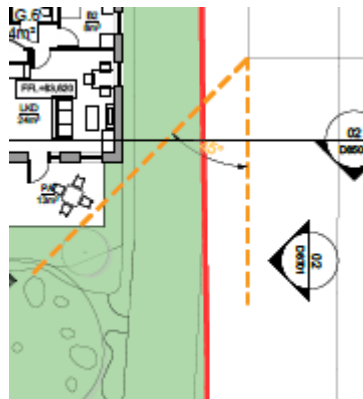


Image J – 45 Degree line taken from corner of no.78

9.28 In terms of privacy, windows and terraces on the front elevation of the development would not cause significant harm to privacy. This is because the nearest residential properties windows that the terraces could face into are in Azalea Court, which is over 20m away. Windows and terraces on the rear elevation would principally have views of flank elevation of nos. 43 to 49 Redbarn Close and over less sensitive rear garden areas of no.78. On the smaller block element flank elevation there are windows at first and second level that face immediately onto more sensitive garden area immediately adjacent to no.78's rear façade. To prevent these windows causing unacceptable loss of privacy, a

condition is recommended requiring them to be obscurely glazed and non-opening up to a height of 1.7m from the internal finished floor level.

- 9.29 The proposed terrace areas would not cause significant noise disturbance to neighbouring properties as they are set a reasonable distance away from neighbouring windows and are modest in size. Generally intensification of the use would not cause unacceptable noise disturbance given the characteristic of the site and the generally low amount of noise generated by residential uses.

Impact on Parking and Highway Conditions

- 9.30 Parking stress as would be expected in an area close to the town centre and train station is very high. There is a controlled parking zone within Purley, however the site lies just outside of this area. There are parking bays on Whytecliffe Road North, however these are unrestricted.

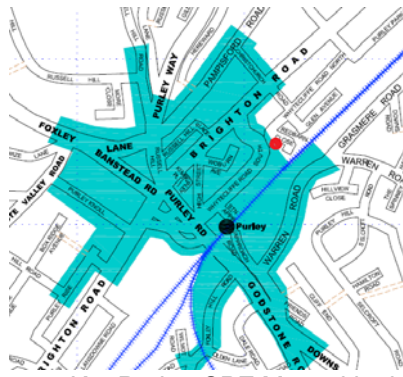


Image K – Purley CPZ Map with site's location shown with red dot.

- 9.31 The site has a PTAL rating of 5, and is close to a wide range of facilities and services. However, it is a location which can support a car free development, but the ability to achieve this is undermined by the lack of parking restrictions on Whytecliffe Road North. To help ensure that the development is sustainable, the applicant has agreed to a £50,000 contribution that will be used to fund a review, consultation on and implementation of the extension of the Purley CPZ in the roads surrounding the development. This is recommended to be secured through the S106.
- 9.32 Whilst due process will need to be followed in regards to the implementation of the CPZ, Officers are satisfied that it is highly probable that the CPZ would be expanded as there is a strong case in its favour. Nevertheless, there is a small risk that CPZ would not be expanded and that this could result in the development increasing parking stress in surrounding local roads. However, this small risk needs to be balanced against the need to deliver housing, with this site in terms of connectivity and other factors, being an optimum location to meet such targets.
- 9.33 A financial contribution has also been sought to implement a car club parking space on surrounding local streets. Car clubs help reduce parking stress by breaking dependency on private car ownership, which also helps promote sustainable modes of transport.

- 9.34 Two on-site disabled car parking spaces are proposed for use by blue badge holders. Whilst this provision is lower than the requirement set out in the Mayor's Housing SPG which requires a space for every M4 (3) residential unit (typically 10% of units), it does comply with policy T6.1 of the draft New London Plan which requires one space per dwelling for 3% of the total dwellings. Given the site's central location, modest site size, high PTAL and the fact that the majority of the local public transport network is fully accessible, the proposed level of disabled car parking provision is acceptable. A condition is recommended to ensure that both car parking spaces are installed with an electric vehicle charging point.
- 9.35 In terms of trip generation, in the worst case scenario the development is forecasted to generate 4 car movements in the morning peak, 3 car movements in the evening peak hour and 30 car movements throughout the day. In addition the residential development is likely to generate 2 daily taxi, 1 Other Goods Vehicle and 5 daily Large Goods Vehicle movements. However, through the implementation of the CPZ expansion and by securing a Travel Plan via S106 agreement, the level of trip generation is likely to be significantly lower. The level of trip generation even in the worst case scenario would have a negligible impact on traffic generation and operation of the highway.
- 9.36 A delivery service plan has been submitted which proposes various management and mitigation measures to ensure that servicing of the site does not have an adverse impact on the operation of the highway. A condition is recommended to secure this.
- 9.37 In regards to cycle parking, the London Plan (2016) requires one long stay cycle space to be provided for every studio/ 1 bed unit, and 2 spaces for every 2 bed+ unit, and 1 short stay space per every 40 units. In total 69 long stay cycle parking spaces and 1 short stay parking space are required by policy. In total 76 cycle parking spaces would be provided within the basement area exceeding policy requirements. Whilst the location of the cycle store is not ideal, given that the store would be secure and a lift provided, no objection is raised. It is not clear from the submission where the short stay cycle parking space would be located and how it would be accessed. Further details of this are recommended to be secured via condition.
- 9.38 To improve the pedestrian environment surrounding the site and the general setting of the development, the applicant has agreed a £25,000 contribution (secured through the S. 106) to fund the creation of a 'pocket park' in the location of the dead end as shown in the image below.



Image L – Dead End to Whytecliffe Road North

- 9.39 A draft travel plan has been submitted which details some of the initiatives in order to ensure that sustainable methods of transport are promoted (such as travel packs for new residents, the promotion of walking initiatives etc.). A full travel plan is recommended to be secured via the S.106 agreement which will be appropriately monitored.
- 9.40 The development will result in both changes to the existing and introduction of new vehicle crossovers. Prior to the first occupation of the development the necessary works to the public highway (undertaken through S.278 of the Highways Act) will be secured via the S.106 agreement.
- 9.41 A draft construction logistics plan (CLP) was submitted with the application. To ensure that the impact of the development during construction does not have an undue impact on the operation of the highway, a full CLP is recommended to be secured via condition.
- 9.42 A refuse store is proposed immediately adjacent to the front entrance, a short distance from the road, thus enabling convenient collection. The bin store contains ten 1,100 litre bins, 4 of which are of recycling. In addition 440 litre of bins for food recycling have been provided. The level of provision is in line with council's guidance and recommended to be secured via condition.

Trees

- 9.43 There are a number of trees on the site and in adjacent gardens, none of which are protected. Nevertheless there are a number of good quality trees. In terms of the trees on the site, the application has sought to retain these trees where possible and integrate them into the landscaping. In total 2 low quality Category C trees would be removed. The application is accompanied by a landscaping plan, which is recommended to be secured via condition. The landscaping plan contains extensive tree planting that would compensate for loss of trees on the site. A condition is recommended to ensure that retained trees are sufficiently protected during the construction of the development.

Flooding

- 9.44 The site is located in Flood Risk Zone 1 as defined by the Environmental Agency, where the annual probability of fluvial and tidal flooding is classified as less than

1 in 1000 years. In terms of surface water, the site has a very low classification which corresponds to annual probability of flooding less than 1 in 1000 years.

9.45 The site is in an area where there is potential for groundwater flooding to occur at surface. The PPG states that ‘the aim should be to keep development out of medium and high flood risk areas (Flood Zones 2 and 3) and other areas affected by other sources of flooding where possible’. The applicant in line with guidance has submitted a sequential test to show whether there are potential development sites with a lower probability of flooding that could be developed instead, to help meet the five year housing supply. The site fails the sequential test as the council can meet their 5 year housing land supply on sites with a lower groundwater flood risk and also in Flood Zone 1.



Image M – Groundwater Flooding Map

9.46 The NPPF states that when it is not possible, following the application of the Sequential Test, for a development to be located in zones with a lower probability of flooding, the Exception Test should be applied. In order to pass the Exception Test the following must be met:

- a) It must be demonstrated that the development provides wider sustainability benefits to the community that outweigh flood risk, informed by a SFRA where one has been prepared; and
- b) A site specific flood risk assessment must demonstrate that the development will be safe for its lifetime taking account of vulnerability of its users, without increasing flood risk elsewhere, and, where possible, will reduce flood risk overall.

9.47 The application complies with part (a) of the Exception Test. The development increases social provision through the delivery of further housing (31% of which would be affordable), in a sustainable location close to local services and transportation links. There are some economic benefits, with employment opportunities being generated through construction that through S106 agreement would directly benefit local people and suppliers. New residents are likely to help the vitality of local shops and economy through the goods and services they purchase. The development has some environmental benefits, with SUDs achieving close to greenfield run-off rates, that would in turn reduce surface water flooding risk both on the site and elsewhere, and through measures in landscaping that could increase biodiversity. The SUDs strategy has been reviewed by the LLFA whom have confirmed that the strategy is in line with their

requirements and acceptable. The SUDs strategy is recommended to be secured via condition.

- 9.48 In regards to (b), a site specific Flood Risk Assessment has been submitted which correctly identifies the proposed flood risk and suggests appropriate mitigation measures that demonstrate that the development will be safe for its lifetime, without increasing flood risk elsewhere. The application passes part (b) and therefore passes the Exception Test.
- 9.49 The application includes a small basement area containing cycle storage and a plant room, in an area at risk of groundwater flooding. In line with paragraph 8.34 of the Croydon Local Plan (2018), a Basement Impact Assessment (BIA) has been submitted. The BIA appropriately reviews comparable local borehole log data, with the conclusion being that the basement level is likely to be in close proximity to the water table. A number of appropriate mitigation measures are proposed including installation of airbricks, using reinforced concrete flooring with a damp proof membrane and installation of Sump pumps. The recommendations of the BIA are recommended to be secured via condition.

Other Planning Issues

- 9.50 Policy SP6.2 requires new development to minimise carbon dioxide emissions, including that new dwellings (in major development proposals) must be Zero Carbon. As a minimum a 35% reduction in regulated carbon emissions over Part L 2013 is required, with the remaining CO₂ emissions to be offset through a financial contribution. The policy also requires major developments to be enabled for district energy connection unless demonstrated not to be feasible.
- 9.51 The proposed development would achieve a 39.5% reduction in regulated CO₂ emissions. The remaining regulated CO₂ emissions shortfall (59 tonnes) would be covered by a carbon offset payment which would need to be secured through a S.106 agreement. There is no district energy connection but a condition is recommended requiring the incorporation of design features to allow the development to make future connection to District Energy Network should one be implemented.
- 9.52 A planning condition is recommended to secure compliance with the domestic water consumption target of 110 litre/person/day, to ensure sustainable use of resources.
- 9.53 London Plan Policy 7.14 (B) states that developments should minimise increased exposure to existing poor air quality and seek to contribute to addressing local air quality problems and Croydon Local Plan (2018) requires development to positively contribute to improving air, land, noise and water quality by minimising pollution. To address this the applicant has submitted a Sustainability Statement (reference 19-E121-003) which has been reviewed by the Council's Environmental Health team and considered acceptable subject to the imposition of conditions. In addition in order to be acceptable a financial contribution is required to be secured via S106 agreement.

In terms of wildlife and biodiversity, the applicant has submitted a Preliminary Ecological Appraisal Report (ref: 18-4730 dated December 2018). The applicant has carried out desktop study, preliminary roost assessment for bats and birds, a phase 1 survey. One further bat study is required for no.68. Whilst no evidence of bats was found within the roof space, there is a theoretical small space between the tiles and the roofing felt that requires further investigation. A condition is recommended requiring this survey to be carried out prior to any material operation. The finding of the report and surveys, which includes a lighting plan, are recommended to be secured via conditions.

- 9.54 In regards to land contamination, a phase 1 Environmental Risk Assessment and a Phase 2: Site Investigation report were submitted in support of the application. Both reports have been reviewed by the council's Land Contamination Officer whom has confirmed that the phase 1 report is satisfactory. However, due to contaminations encountered during the initial site investigations, further ground gas monitoring is required. These further investigations are recommended to be secured via condition, along with a Remediation Strategy.
- 9.55 In line with policy DM16 of the Croydon Local Plan (2018) a health impact assessment was submitted which identifies that the proposal will improve housing quality, has good access to health, social and retail facilities, open space, and would be environmentally sustainable. Planning obligations and conditions are recommended restricting car use to avoid unacceptable health impacts.
- 9.56 Croydon Local Plan policy SP3.14 and the Planning policy including the adopted Section 106 Planning Obligations in Croydon and their Relationship to the Community Infrastructure Levy – Review 2017 sets out the Councils' approach to delivering local employment for development proposal. A financial contribution and an employment and skills strategy would be secured as part of the legal agreement.
- 9.57 The development would be liable for both Mayoral Community Infrastructure Levy (CIL) and Croydon CIL. The collection of CIL would contribute to provision of infrastructure to support the development including provisions, improvement, replacement, operation or maintenance of education facilities, health care facilities, and opens space, public sports and leisure, and community facilities.

10.0 Conclusion

- 10.1 All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted for the reasons set out above. The details of the decision are set out in the RECOMMENDATION.

Appendix 1: Planning Policies and Guidance

The following lists set out the most relevant policies and guidance, although they are not exhaustive and the provisions of the whole Development Plan apply (in addition to further material considerations).

London Plan (2016)

- Policy 1.1 Delivering the Strategic Vision and Objectives for London
- Policy 2.6 Outer London: vision and strategy
- Policy 2.8 Outer London: transport
- Policy 2.18 Green Infrastructure
- Policy 3.1 Ensuring equal life chances for all
- Policy 3.2 Improving health and addressing health inequalities
- Policy 3.3 Increasing housing supply
- Policy 3.4 Optimising housing potential
- Policy 3.5 Quality and design of housing developments
- Policy 3.6 Children and young people's play and informal recreation facilities
- Policy 3.8 Housing choice
- Policy 3.9 Mixed and balanced communities
- Policy 3.10 Definition of affordable housing
- Policy 3.11 Affordable housing targets

- Policy 3.12 Negotiating affordable housing on individual private residential and mixed use schemes
- Policy 3.13 Affordable housing thresholds
- Policy 5.1 Climate change mitigation
- Policy 5.2 Minimising emissions
- Policy 5.3 Sustainable design & construction
- Policy 5.7 Renewable energy
- Policy 5.8 Innovative energy technologies
- Policy 5.9 Overheating and cooling
- Policy 5.10 Urban greening
- Policy 5.11 Green roofs and development site environs
- Policy 5.12 Flood risk management
- Policy 5.13 Sustainable drainage
- Policy 5.14 Water quality and wastewater infrastructure
- Policy 5.15 Water use and supplies
- Policy 5.16 Waste net self-sufficiency
- Policy 5.17 Waste capacity
- Policy 5.18 Construction, excavation and demolition waste
- Policy 5.21 Contaminated land
- Policy 6.1 Strategic Approach
- Policy 6.3 Assessing Effects of Development on Transport Capacity
- Policy 6.5 Funding Crossrail and other strategically important transport infrastructure
- Policy 6.7 Better streets and surface transport
- Policy 6.9 Cycling
- Policy 6.10 Walking
- Policy 6.11 Smoothing traffic flow and tackling congestion
- Policy 6.12 Road network capacity
- Policy 6.13 Parking
- Policy 7.1 Lifetime neighbourhoods
- Policy 7.2 An inclusive environment
- Policy 7.3 Designing out crime
- Policy 7.4 Local character
- Policy 7.5 Public realm
- Policy 7.6 Architecture
- Policy 7.14 Improving air quality
- Policy 7.15 Reducing noise and enhancing soundscapes
- Policy 7.19 Biodiversity and access to nature
- Policy 7.21 Trees and woodlands
- Policy 8.1 Implementation
- Policy 8.2 Planning obligations
- Policy 8.3 Community infrastructure levy

Croydon Local Plan (2018)

Strategic Policies

- Policy SP1: The Places of Croydon
- Policy SP2: Homes
- Policy SP3: Employment
- Policy SP4: Urban Design and Local Character
- Policy SP6: Environment and Climate Change
- Policy SP7: Green Grid
- Policy SP8: Transport and Communication

Development Management Policies

- Policy DM1: Housing choice for sustainable communities
- Policy DM10: Design and character
- Policy DM13: Refuse and recycling
- Policy DM14: Public Art
- Policy DM16: Promoting Healthy Communities
- Policy DM18: Heritage assets and conservation
- Policy DM23: Development and construction
- Policy DM24: Land contamination
- Policy DM25: Sustainable Drainage Systems and Reducing Flood Risk
- Policy DM27: Protecting and enhancing our biodiversity
- Policy DM28: Trees
- Policy DM29: Promoting sustainable travel and reducing congestion
- Policy DM30: Car and cycle parking in new development

Place-specific policies

- Policy DM42: Purley

Supplementary Planning Guidance (SPG) / and Documents (SPD)

London Plan

- Affordable Housing & Viability (August 2017)
- Crossrail Funding (March 2016)
- Housing (March 2016)
- Accessible London: Achieving an Inclusive Environment (October 2014)
- The control of dust and emissions during construction and demolition (July 2014)
- Character and Context (June 2014)
- Sustainable Design and Construction (April 2014)
- Play and Informal Recreation (September 2012)
- Planning for Equality and Diversity in London (October 2007)

Croydon Development Plan

- Suburban Design Guide 2019 SPD
- Designing for community safety SPD
- SPG 12: Landscape design

Appendix 2: BRE Guidance Terms

Daylight to existing buildings

The BRE Guidelines stipulate that the diffuse daylighting of the existing building may be adversely affected if either:

- the vertical sky component (VSC) measured at the centre of an existing main window is less than 27%, and less than 0.8 times its former value (or reduced by more than 20%), known as “the VSC test” or
- the area of the working plane in a room which can receive direct skylight is reduced to less than 0.8 times its former value known as the “daylight distribution” test.

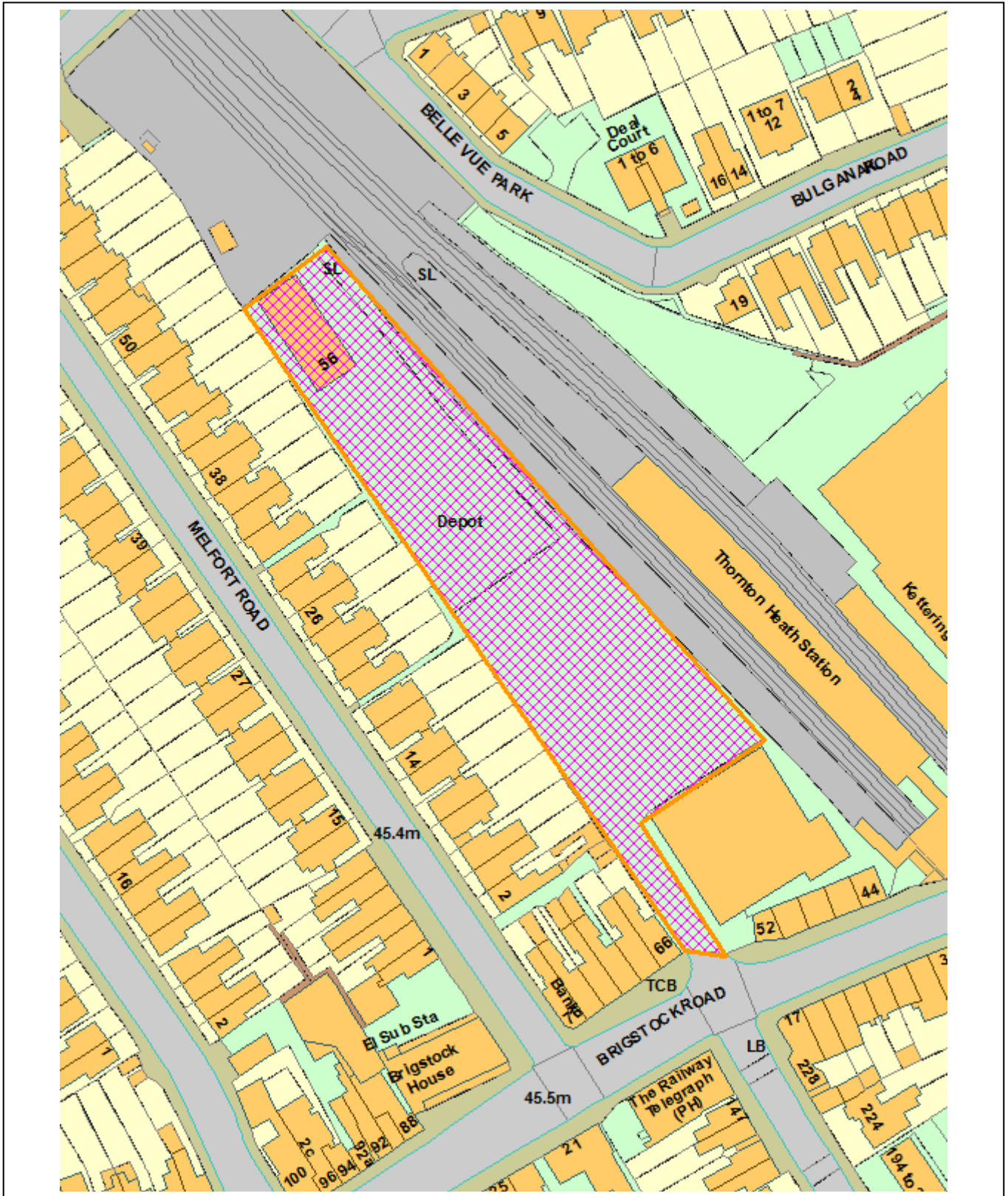
Sunlight to existing buildings

The BRE Guidelines stipulate that the sunlight of an existing window may be adversely affected if the centre of the window:

- receives less than 25% of annual probable sunlight hours (APSH), or less than 5% of annual winter probable sunlight hours between 21 September and 21 March (WPSH); and
- receives less than 0.8 times its former sunlight hours (or a 20% reduction) during either period; and
- has a reduction in sunlight received over the whole year greater than 4% of annual probable sunlight hours.

If one of the above tests is met, the dwelling is not considered to be adversely affected.

This page is intentionally left blank



This page is intentionally left blank

Officer Report:**PLANNING COMMITTEE AGENDA**21st November 2019**PART 6: Planning Applications for Decision****Item 6.2****1.0 SUMMARY OF APPLICATION DETAILS**

Ref: 18/05856/FUL
 Location: Station Yard, 56 Brigstock Road
 Ward: Bensham Manor
 Description: Demolition of existing builders yard buildings, erection of a block comprising a 6 storey building and a part 7, 8 and 9 storey building comprising 58 residential units (26 x 1 bed, 9 x 2 bed and 23 x 3 bed apartments) and two commercial units (Use Class B1c) along with the provision of associated off-street parking and refuse and cycle storage.

Drawing Nos: 17 165_61 ; 17 165_11 ; 17 165_12E ; 17 165_14D ; 17 165_15C ; 17 165_16C ; 17 165_17C ; 17 165_18C ; 17 165_19C ; 17 165_20P1 ; 17 165_21E ; 17 165_22E ; 17 165_23 E ; 17 165_24 E ; 17 165_25D ; 17 165_26D ; 17 165_27 ; 17 165_31A ; 17 165_41A ; 17 165_42B ; 17 165_51P1 ; 17 165_51P2 ; 17 165_52P1 ; 17 165_52P2 ; 17 165_53 P1 ; 17 165_53P2 ; 17 165_55P1 ; 17 165_56P1.

Applicant: Kenexa Estates (Thornton Heath) Ltd
 Agent: Mr Alex Bateman, SHW LLP
 Case Officer: Dean Gibson

Proposed Residential Accommodation

	Market	Shared	Rented	Total
1 bed	13	6	7	26 (45%)
2 bed	3	3	3	9 (15%)
3 bed	11	5	7	23 (40%)
Total	27 (47%)	14 (24%)	17 (29%)	58

Affordable – 31 (53%) dwellings on site: 17 (29%) London affordable rented dwellings and 14 (24%) shared ownership dwellings with required early and late stage review mechanisms. The affordable housing split based on habitable rooms would be 55% affordable rent provision and 45% shared ownership provision.

Proposed Employment Use

	Number of Units	Total Floor Area	Net Change
Employment (B1c Use)	2	153.20	-160.8

Parking and Cycle Storage Provision

	Number of car parking spaces	Number of Disabled Spaces	Number of cycle parking spaces
Residential	2	2 car spaces	100 Residential 18 Visitor

1.1 This application is being reported to Planning Committee because the Ward Councillor (Alison Butler) has made representations in accordance with the Committee Consideration Criteria and requested Planning Committee consideration. Moreover, objections above the threshold in the Committee Consideration Criteria have been received.

2.0 RECOMMENDATION

2.1 That the Planning Committee resolve to GRANT planning permission prior to the completion of a legal agreement to secure the following:

- a) Affordable housing – 31 dwellings (17 x London affordable rent at and 14 x London Shared Ownership) with review mechanisms;
- b) Local Employment and Training contribution (£35,835 – Comprising £31,000 for construction phase and £4,835 for end user phase) and compliance with employment and training and local business initiatives;
- c) Financial contribution towards air quality (£5,800);
- d) Provision of on-site car club space
- e) Future restriction of car parking permits
- f) Marketing strategy for commercial units.
- g) Carbon off-setting financial contribution (£76,093)
- h) Monitoring fees

2.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the legal agreement indicated above.

2.3 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. The development shall commence within 3 years of the date of planning permission.
2. Development to be carried out in accordance with the approved drawings and reports except where specified by conditions.
3. Details and samples of materials to be submitted.
4. Detailed elevational drawings (Scale 1:10) showing window reveals.
5. Details of signage (to commercial units) to be submitted.
6. Details of boundary treatment and retaining walls to be submitted.
7. Details of children's play-space to be submitted for approval.

8. Security lighting of bin and bike stores, surface and parking areas to be submitted for approval.
9. Inclusive disabled access M4(2) and M4(3) to be achieved.
10. Car club space on site to be provided prior to first occupation.
11. Details of electric vehicle charging points (active and passive) to be submitted.
12. Full details of cycle storage enclosure appearance to be submitted.
13. Full details of refuse storage enclosures appearance to be submitted.
14. Construction Logistics Plan to be submitted.
15. Actionable measures of Travel Plan as submitted to be adhered to.
16. BREEAM Excellent (Commercial units)
17. 110L Water usage (Residential units)
18. Construction environmental management plan to be submitted prior to any development on site.
19. Development to be completed in accordance with mitigation outlined in Noise Assessment.
20. Details of sound insulation between commercial and adjacent ground and first floor dwellings to be submitted.
21. Details of sound proofing/noise attenuation measures to residential flats in the development.
22. The use of the commercial units to be provided/retained as hub shared workspaces.
23. SUDS and drainage details (In accordance with Flood Risk Assessment) to be submitted.
24. Contaminated Land - Remediation strategy to be submitted.
25. Details of connection to foul and/or surface water drainage system to be submitted.
26. No infiltration of surface water drainage into the ground.
27. No piling or any other foundation designs using penetrative methods without consent of the LPA.
28. Hours of operation of Commercial Units.
29. Details of noise limiters to Commercial Units.
30. Public art details to be submitted.
31. Delivery and Servicing Plan to be submitted.
32. Secure by Design details to be submitted.
33. Details of security shutters to commercial units to be submitted.
34. No windows to be provided other than those shown on approved plans.
35. Submission of full landscaping details including to roof terraces.
36. Submission of evidence that Network Rail have agreed/approved the following matters :- asset protection agreement, adherence to ASPRO guidance, Design and RAMS for permanent / temporary work affecting railway operation, maintenance of access to Network Rail storage yard at construction and post-construction phases.
37. Submission of details of aviation warning lighting to cranes during construction phase.
38. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Granted subject to a Section 106 Agreement
- 2) Community Infrastructure Levy
- 3) Code of practise for Construction Sites
- 4) Light pollution
- 5) Requirement for ultra-low NOx boilers
- 6) Nesting birds in buildings
- 7) TfL Informative – Mayor’s Vision Zero initiative.
- 8) Thames Water informative
- 9) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

2.4 That the Committee confirms that adequate provision has been made by the imposition of conditions for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

2.5 That if by 22nd February 2020 the legal agreement has not been completed, the Director of Planning and Strategic Transport is delegated authority to refuse planning permission.

3.0 PROPOSAL AND LOCATION DETAILS

3.1 The planning proposal seeks full planning permission.

3.2 The proposal includes the following:

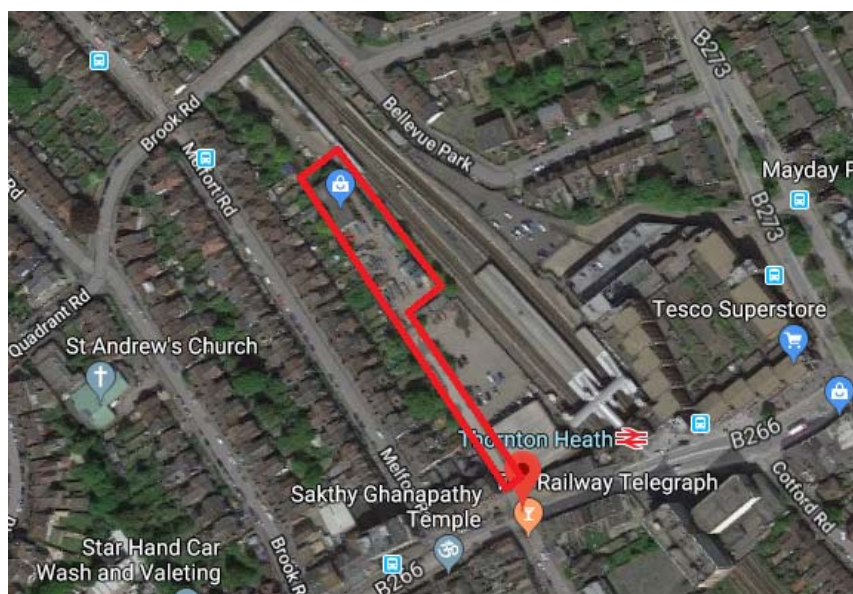
- Demolition of existing buildings.
- Erection of a two blocks – one 6 storey block and one part 7, 8 and 9 storey block.
- Provision for 153 square metres of B1 floorspace at ground floor level.
- Provision of 58 residential flats
- 2 car parking spaces
- Provision of associated cycle storage and refuse storage.
- Provision of communal external amenity spaces and children's play space

3.3 During the course of the application amended plans have been received. The main alterations to the schemes design have been as follows:

- Number of Flats 58 remained unchanged, but with changes to the residential mix
 - Number of 3 bedroom flats is 23 (previously 2)
 - Number of 2 bedroom flats is 9 (previously 22)
 - Number of 1 bedroom flats is 26 (previously 24)
- Number of parking space reduced from 10 space to 2 spaces (more green space/buffer space proposed)
- Ground floor employment space in smaller block relocated to the ground floor of larger block (with part of ground floor accommodation (2 flats) moving to smaller block.

- 3.5 The site falls just outside the boundary of the Thornton Heath District Centre; the car park to the south of the application site and the associated Iceland store is included within the District Centre). Moreover, whilst the site would nominally be designated as a Tier 4 'scattered employment site', in this instance the site has a specific site allocation (Site Allocation 136) which also includes the Iceland store and associated car park). The allocation seeks to retain some employment whilst making more efficient use of the site by providing homes that will meet the boroughs need for housing and a replacement unit. Indicative housing numbers suggest the site has capacity for between 25-55 homes.
- 3.6 The site is bounded to the south-east by a 24 hour controlled car park serving an Iceland supermarket.
- 3.7 The site is bounded to the north-east by railway tracks leading directly into Thornton Heath railway station. The site is approximately 200 metres from the railway station ticket- office.
- 3.8 The site is bounded to the north-west by a railway goods yard – with a vehicular access between the railway tracks and the north-eastern boundary of the site.
- 3.9 The site is bounded to the south-west by two storey terraced houses at 20 to 46 Melfort Road. There is a high retaining perimeter brick boundary wall separating the site from the rear gardens of those houses which means that the rear gardens of Melfort Road properties are at a higher level (compared to the application site).
- 3.10 In the vicinity of the site are several tall buildings including the Tesco supermarket/Laxton Court and Ambassador House to the south-west.
- 3.11 The site itself is not subject to any heritage designations, although the Clocktower at the intersection of Brigstock Road/Parchmore Road/High Street is classified as a Local Designated View.

Fig 3. Aerial view highlighting the proposed site within the surrounding area



- 3.12 The application site has a PTAL of 5 and is within a 2 minute walk from Thornton Heath Railway Station. The site is located adjacent to the boundary of Thornton Heath District Centre. The area is subject to a Controlled Parking Zone and Brigstock Road and Melfort Road are classified as local distributor roads.
- 3.13 The site is also captured and influenced by the Thornton Heath Place Specific Policy which advises that development should complement existing predominant building heights of 3 storeys up to a maximum of 4 storeys except on the vicinity of Thornton Heath Railway station where any tall or large buildings proposed should not exceed 9 storeys.

Planning History

- 3.14 Pre-Application Enquiry (LBC Ref 18/02930/PRE) for residential development – 32 flats.

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- Whilst the application has not been accompanied by two year marketing evidence, the site allocation overrides this requirement to a certain extent and in accordance with the allocation, the scheme provides some replacement employment floorspace alongside a reasonable level of residential accommodation, including affordable housing (embracing mixed tenures). This is considered acceptable under these specific circumstances.
- The principle of intensive residential development is acceptable given the national and local need for housing and the brownfield status of the land. The development would make a significant contribution towards meeting borough-wide housing targets.
- The proposal would provide 53% on-site affordable housing provision (a mix of London Affordable Rent and Shared Ownership alongside required early and late stage review mechanisms) in accordance with local plan requirements. Officers are satisfied with the 55% affordable rent to 45% shared ownership habitable room tenure split.
- The proposal would deliver a significant number of family units, with 67% of units being suitably sized for families/small families (2 and 3 bed/4 person units).
- The development would have an acceptable impact on the nearby designated local view – Clocktower - with no harm being caused to this heritage asset. There are no other heritage designations in place.
- The scheme has been amended to render the design and appearance of the development acceptable. Whilst it is acknowledged that the proposed height would be at variance with the existing structures on site, the massing, height, appearance and form of the development would be in context with nearby tall buildings and would represent a striking and appropriate back-drop to the adjacent railway tracks – with opportunities to enhance the relationship between the application site and its adjacent sites and would not restrict or compromise the wider re-development of the allocated site (including the adjacent car park and Iceland Store) should this opportunity arise in the future.

- The living conditions of adjacent occupiers would be protected from undue harm, subject to the use of planning conditions.
- The living standards of future occupiers would be acceptable (in terms of overall residential quality) and would comply with the Nationally Described Space Standard (NDSS).
- The level of parking and impact upon highway safety and efficiency would be acceptable, in view of high public transport accessibility levels and close proximity to a District Centre location.
- Sustainability aspects have been properly assessed and their delivery can be controlled through planning obligations and planning conditions.
- Secure by Design principles can be secured subject to condition.

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

Greater London Authority (GLA)

5.2 The GLA were consulted (and re-consulted on amended plans) due to the height of the proposed 9 storey building exceeding 30 metres in height.

5.3 The London Mayor strongly supported the delivery of 53% affordable housing – via the fast track route - in accordance with the strategic target set by policies 3.11, 3.12 of the London Plan and policies H5 and H6 of the draft London Plan – subject to the use of an early stage viability review should the scheme not progress within 2 years from the date of planning permission.

5.4 The London mayor was also satisfied that the proposed design, internal configuration and layout would not have presented a strategic design concern. He recommended that the Council should secure key details of facing materials to ensure a high quality building is delivered.

5.5 He concluded that the scheme would be acceptable in strategic planning terms and advised that the Council should progress to determine the planning application without any need to refer to the GLA (at Stage 2).

Lead Local Flood Authority (LLFA) (Statutory Consultee)

5.6 The LLFA has no objection to the development subject to a condition requiring compliance with the submitted Flood Risk Assessment & Surface Water Drainage Strategy and further information being submitted for approval with regard to the detailed designs of a surface water drainage scheme.

Transport for London

5.7 No objection. Supports car free development. Requires construction logistics plan and delivery and servicing plan to be secured by condition.

Network Rail

5.8 No objection, provided that the developer will:

1. Sign asset protection agreement with Wessex ASPRO before proceeding with any design/construction works at the site within the NR zone of influence;
2. Adopt ASPRO guidance and requirements that will be provided to the developer in kick-off meeting;
3. Submit for our acceptance the Design and RAMS for any permanent or temporary work that could affect NR assets and railway operation.
4. Ensure that NR current access to NR storage yard will not be affected during construction and post-construction phase.

Thames Water

5.9 Thames Water does not have any objection but makes good practice recommendations on waste water and water pressure. The recommendations can be passed to the developer through an informative.

Metropolitan Police – Crime Prevention Officer

5.10 No objection subject to a ‘Secured by Design’ condition being attached to any planning permission to ensure the development would follow the principles and physical security requirements of Secured by Design.

6.0 LOCAL REPRESENTATION

6.1 The application has been publicised by way of 102 letters of notification to neighbouring properties in the vicinity of the application site. Site notices were also displayed in the vicinity of the site and a press note published. Following the receipt of amended plans in May 2019, notification letters were re-issued and amended site notices were displayed advising neighbouring properties of the amendments.

6.2 The number of representations received from neighbours in response to notification and publicity of the application are as follows:

No of individual responses: 27
Objecting: 22 Supporting: 2 Commenting: 3

6.3 The following issues were raised in representations. Those objections that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Objection	Officer Response
------------------	-------------------------

<p>Proposal represents a piecemeal development and an overdevelopment of the site with increased pressure on local amenities/services.</p>	<p>The applicant has submitted an Illustrative Masterplan to demonstrate how the adjacent parcels of land could be developed in a cohesive and sensitive manner.</p> <p>The density of development has been properly assessed and is within the density ranges outlined in the London Plan. Moreover, the form, scale and intensity of development would be appropriate, with effects either being acceptable or suitably mitigated through the use of planning conditions and/or planning obligations.</p> <p>The development would be subject to the Community Infrastructure Levy which would assist in contributing towards the improvement of local amenities/services.</p>
<p>The development would be out of character with the surrounding area</p>	<p>Bearing in mind the close relationship to Thornton Heath Railway Station and nearby taller buildings within the District Centre, officers are content with the overall scale of development and consider it to be in keeping with the character and appearance of the area. The Place Specific policy for Thornton Heath recognises that there is scope for taller buildings (up to 9 storeys) in close proximity to the railway station.</p>
<p>Increased pressure being placed on car parking within surrounding streets.</p>	<p>The site is located in a sustainable location and the provision of less than 1-1 car parking is acceptable in such circumstances. The site is in close walking distance from the District Centre amenities, Thornton Heath Railway Station and local bus routes. Limitations around access to any future car parking permits is recommended for inclusions as part of a S.106 planning obligation along with the provision of a car club space.</p>
<p>Poor Access</p>	<p>The access to the site is existing and the proposal would bring about significant improvements to its</p>

	appearance. The site is part of a proposal site and the applicant has submitted an Illustrative Masterplan to determine how the access could serve future development of the proposal site.
Insufficient amenity space	Officers are content that the proposed surface level landscaped garden spaces, private balconies and communal roof terraces areas would provide an acceptable amount of amenity space/play space within the development. It would allow for the provision of tree planting and measures to promote enhanced biodiversity.
Increased Crime/Anti-Social Behaviour	A secure by design condition is recommended in line with comments from the Metropolitan Police Crime Prevention officer.
Neutral Comments	Officer Response
Emergency Vehicle Access	Officers are content that emergency vehicles would be able to enter and exit the site in a forward gear.
Noise from railway may impact future occupiers.	An independent noise survey was submitted with the application and concluded that noise from the adjacent railway land would be acceptable subject to the provision of appropriate glazing.
Preparing petition against application.	No petition was received.
Supporting Comments	
Support the development.	

6.4 The following Councillor has made representation:

Councillor Alison Butler (Ward Councillor). Objecting and referred application to committee on the following grounds:

- Development requires a master planned approach for the whole site.
- Development should reflect the character of the area and be a landmark building.
- Poor design quality.
- Lacks amenity space and particularly play space for children.
- Access to the development requires further consideration.

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Meeting the challenge of climate change.
- Delivering a sufficient supply of homes.
- Building a strong, competitive environment.
- Promoting sustainable transport.
- Making effective use of land.
- Achieving well designed places.

7.3 The planning policies raised by the application that the Committee are required to consider are listed below and are broadly examined in the Planning Considerations section of this report.

Consolidated London Plan 2015

- 3.1 Ensuring equal life chances for all
- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.6 Children/young person's play & informal recreation areas
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 3.11 Affordable housing targets
- 3.12 Negotiating affordable housing
- 3.13 Affordable housing thresholds
- 4.4 Managing industrial land and premises
- 5.1 Climate change mitigation

- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture
- 7.14 Improving air quality
- 7.19 Biodiversity and access to nature
- 7.21 Woodlands and trees

Croydon Local Plan 2018

- SP1 The Places of Croydon
- DM48 Thornton Heath
- SP2 Homes
- DM1 Housing choice for sustainable communities
- SP3 Employment
- SP3.2 Innovation, investment and enterprise
- SP4 Urban design and local character
- DM10 Design and character
- DM13 Refuse and recycling
- DM14 Public art
- DM15 Tall buildings
- DM16 Promoting healthy communities
- DM18 Heritage assets and conservation
- DM16 Promoting healthy communities
- SP5 Community facilities
- DM19 Protecting and providing community facilities
- SP6 Environment and climate change
- SP6.3 Sustainable design and construction
- DM23 Development and construction
- DM25 Sustainable drainage systems and reducing floor risk
- SP7 Green Grid
- DM27 Biodiversity
- DM28 Trees
- SP8 Transport and communications
- DM29 Promoting sustainable travel and reducing congestion
- DM30 Car and cycle parking in new development

7.4 There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016

- Croydon Suburban Design Guide Supplementary Planning Document April 2019

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the Planning Committee should consider are as follows:

1. Principle of Development – Including Employment Considerations
2. Affordable Housing and Housing Mix
3. Townscape, Visual Impact and Heritage Impact
4. Housing Quality for Future Occupiers
5. Residential Amenity for Neighbours
6. Parking and Highway Safety
7. Flood Risk
8. Sustainability
9. Trees, Landscaping and Biodiversity
10. Other Planning matters

Principle of Development

Loss of Employment Use

- 8.2 The application site is located adjacent to Thornton District Centre and is designated as a Tier 4 employment site. Planning policy therefore provides protection for existing industrial activities and supports incoming uses that fall within the B1b/B1c, B2, B8 use classes as well as related sui generis uses. It also makes provision for D1 uses comprising education and community facilities. That said, the presence of Site Allocation 136 recognises that mixed use redevelopment of the site would be acceptable, with the replacement of some employment use. This to a certain extent over-rides the need to submit full marketing evidence to justify the loss of the existing scaffold yard and it is significant that the allocation gives only limited indication as to the level of replacement floorspace that would be necessary to ensure compliance with the site allocation.
- 8.3 In this instance, the site has historically been used for storage purposes and the existing floorspace on site amounts to 314 square metres.
- 8.5 Whilst evidence has not been submitted to justify the loss of existing accommodation and/or an alternative exclusive employment-focussed redevelopment scenario, the proposal would still re-provide 153.20 square metres of employment-related floorspace (providing a range of uses B1 uses encouraged by Policy SP3.2). Officers accept that the proposed floorspace might well provide opportunities to increase employment on site, especially if the accommodation is used flexibly and innovatively. In his comments, the London Mayor accepted that the site was physically constrained in such a way that restricts it from providing the necessary yard space required to successfully support a co-located industrial use without compromising other planning priorities

for the site – as set out in the site allocation (specifically the delivery of new homes/affordable homes).

- 8.6 Therefore, whilst the level of employment floorspace will be reduced and very limited evidence has been submitted to either justify the loss of existing accommodation and/or the capacity deliver a single employment-related or community use, the re-provision of commercial units (laid out to meet modern work practices as part of a mixed use redevelopment scenario) would help deliver high quality replacement employment-related floorspace. This accommodation would allow for more effective, flexible and efficient commercial floorspace which should lead to more jobs being made available. Consequently, officers are satisfied with the decrease of the existing employment floorspace, especially as this proposed development scenario should significantly contribute to the on-going regeneration of district centres across the borough.
- 8.7 A planning condition is recommended to ensure that the commercial uses are restricted to B1 uses specifically purposed as hub type shared workspaces as there is a known demand for such accommodation in the vicinity. Moreover, full marketing of the commercial and community related accommodation will be required (on commencement of development) which will be required and managed through the S.106 Agreement. Future employment opportunities for local people at construction and operational phases would be captured as part of a Local Employment Training Strategy – to be contained within a subsequent S.106 Agreement.

New Housing

- 8.8 This proposed development should be viewed against a backdrop of significant housing need, not only in Croydon but also across London and the South-East. All London Boroughs are required by the London Plan to deliver a number of residential units within a specified plan period. In the case of Croydon, there is a requirement to deliver a minimum of 32,890 new homes between 2016 and 2036. Croydon's actual need identified by the Croydon Strategic Housing Market Assessment is an additional 44,149 new homes by 2036, but at that time, there was considered to be limited developable land available for residential development in the built up area. This requirement is set out in policy SP2.2 of the Croydon Local Plan (CLP) (2018), which separates this target into three relatively equal sub targets with 10,760 new homes to be delivered within the Croydon Opportunity Area, 6,970 new homes as identified by specific site allocations for areas located beyond the Croydon Opportunity Area boundary and 10,060 homes delivered across the Borough on windfall sites. In order to provide a choice of housing for people in socially-balanced and inclusive communities in Croydon, the Council will apply a presumption in favour of sustainable development of new homes.
- 8.9 This housing growth expectation includes Thornton Heath which is defined by the 'Places of Croydon' section of the CLP (2018) as being an area '*... with some opportunity for windfall sites*'. It goes on to state that '*Within the Thornton Heath District Centre and its environs, to ensure a balance is struck between strengthening and enhancing the character and enabling growth, proposals*

should complement the existing predominant building heights of 3 storeys up to a maximum of 4 storeys except in the vicinity of Thornton Heath railway station where any tall or large buildings proposed should not exceed 9 storeys. In this particular instance the site benefits from being part of an allocated proposal site for mixed use development that seeks to make more efficient use of the site by providing homes that will help meet the borough's homes target.

8.10 The application is for a mixed use employment and residential development providing new employment opportunities and additional homes within the borough. The scheme has the potential significant regenerative benefits for the immediate area. The site is located within an existing residential area with a close relationship with the neighbouring District Centre and as such, providing that the proposal accords will all other relevant material planning considerations, the principle of development is supported.

Affordable Housing and Housing Mix

Affordable Housing

8.11 The CLP (2018) states that to deliver affordable housing in the Borough on sites of ten or more dwellings, the Council will negotiate to achieve up to 50% affordable housing (subject to viability) and will seek a 60:40 ratio between affordable rented and intermediate homes (including shared ownership) unless there is an agreement with a Registered Provider that a different tenure split can be justified. CLP Policy SP2.5 requires a minimum provision of affordable housing to be provided either:

- preferably as a minimum level of 30% affordable housing on the same site as the proposed development or, if 30% on site provision is not viable;
- a minimum level of 15% affordable housing on the same site as the proposed development, plus a review mechanism entered into for the remaining affordable housing (up to the equivalent of 50% overall provision through a commuted sum based on a review of actual sales values and build costs of completed units) provided 30% on site provision is not viable and construction costs are not in the upper quartile.

8.12 The applicant opted to take a fast-track route to viability assessment (as determined by the London Mayor's Affordable Housing SPD) known as the 'threshold approach'. Under the threshold approach, applicants are not required to provide viability information, nor be subject to review mechanisms provided an agreed level of progress is made following the grant of planning permission, where they:

- deliver at least 35 per cent affordable housing on-site without public subsidy; are consistent with the relevant tenure split:
- meet other obligations and requirements to the satisfaction of the LPA and the Mayor where relevant and

- have sought to increase the level of affordable housing beyond 35 per cent by accessing grant.

8.13 If the scheme fails to be actively progressed within 2 years of the date of the planning permission, an Early Stage Viability Review will be triggered – to determine whether further affordable housing might be possible (or a modification to the tenure mix). If such a review is triggered, changes in values and build costs will be determined at the point of review.

8.14 The applicant has confirmed that 53% of the proposed accommodation (by habitable room) would be provided as affordable housing (55% of which would be delivered as London Affordable Rent and 45% as Shared Ownership). Review mechanisms will also need to be incorporated to respond to the threshold approach and changes to economic circumstances. The level of affordable housing to be delivered (at this stage) would therefore equate to 31 dwellings; 17 affordable rented dwellings and 14 shared ownership dwellings. Based on the threshold approach in London Plan the proposed affordable housing would be acceptable and in accordance with emerging policy requirements set out in the Policies H6 and H7 of the draft London Plan July 2019 as being an acceptable level of affordable housing (at this stage of the development process). The proposed affordable housing and review mechanisms would be managed through the recommended S.106 Agreement..

Housing Mix

8.15 Policy SP2.7 seeks to ensure that a choice of homes is available to address the borough's need for homes of different sizes; achieved by setting a strategic target for 30% of all new homes up to 2036 to have three or more bedrooms. Policy DM1.1 requires a minimum provision of homes designed with 3 or more bedrooms on sites of 10 or more dwellings. In urban locations with a PTALs of 5 (such as this) the requirement is for 40% 3+ bedroom units.

8.16 The development proposes a unit mix comprising of 26x1 bedroom (2 person) dwellings, 9x2 bedroom (4 person) dwellings and 23x3 bedroom (5 person) dwellings. In this case the proposed 23x3 bedroom dwellings would equate to 40% provision which would be in compliance with policy. When including the 2 bedroom (4 person) units, the scheme would provide 55% family sized dwellings. Therefore, over half the proposed dwellings would result in family sized accommodation.

Townscape and Visual Impact and Heritage Impact

8.17 Policy DM48.1 of the CLP states that to facilitate growth and to enhance the distinctive character of Thornton Heath, developments should complement the existing predominant building heights of 3 storeys up to a maximum of 4 storeys except in the vicinity of Thornton Heath railway station where any tall or large buildings proposed should not exceed 9 storeys.

- 8.18 The existing buildings on the site have functional warehouse appearances and are of little architectural or historic importance. There is no objection to their demolition/removal.

Figure 4. Aerial Visual of Proposed Scheme



Density Considerations

- 8.19 The site has an urban setting with a PTAL rating of 5 and as such the London Plan indicates that the density levels ranges of 200-700 habitable rooms per hectare (hr/ha) would be appropriate.
- 8.20 The proposal would have a residential density of 855/hr/ha and whilst this would fall outside the density range suggested for this urban situation (with a relatively high PTAL) the scheme would have an open outlook over the railway tracks with the height of buildings and scale of development respecting the local plan approach and the wider growth and regenerative agenda. In any case and as members will be aware, the London Plan density matrix should not be used mechanistically, with officers preferring to adopt a design lead approach to determining an appropriate density.
- 8.21 The application site is within an established residential area and its footprint would be broadly comparable in size to other flatted developments in the vicinity; and the impact on local character is considered to be acceptable (which is further considered in the following paragraphs). The impact of the development on the neighbouring highway network (including on and off street car parking capacity) and railway lines would also be acceptable, as further highlighted below. The proposal would result in a development that would have an acceptable impact on the appearance of the street scene and the District Centre more generally and would accord with the national and local requirements to intensify the development potential of sites and to optimise the delivery of additional housing in a sustainable manner.

Figure 5 – Aerial Visual of Proposed Scheme



Townscape and Appearance

- 8.22 The proposed layout would provide a linear form with two separated masses, a singular 6 storey tower and a block comprising 7, 8, and 9 storey towers which would be sighted broadly at right angles to each-other. The 6 storey tower would have a south-east/north west orientation with the 7 to 9 storey tower having a north-east/south-west orientation. An existing pedestrian/vehicle access from Brigstock Road would serve the site. The 6, 7, and 9 storey towers would be fully residential, respectively comprising 12, 16, and 18 dwellings in each element – each utilising a separate core each. The 7 storey tower would have 2 commercial units at ground floor and 12 dwellings on the upper floors – again served by a separate core. Each of the towers would have landscaped communal roof terraces and buffer soft landscaping areas would also be provided at ground level as well as a new paved vehicle and pedestrian access. The proposed development would be a significant improvement to the existing site compared to the current storage buildings and dilapidated ground surface.
- 8.23 The tower nearest to Brigstock Road would rise up to 9 storeys height whilst the singular tower closest to the rear of the site would rise up to 6 storeys. The layout of dwellings in these buildings has been arranged to maximise outlook by providing dual aspects. The proposed 7 storey tower would provide employment uses at ground floor level.

Figure 6 – West Elevation View



Figure 7 – Visual of a Roof Terrace



8.24 While one of the towers would rise up to 9 storeys in height, Policy DM41 makes provision for such building height in the environs of the Thornton Heath District Centre and its railway station. The height of the development would be in context with the series of taller buildings located in the vicinity of the site in Brigstock Road, notably Ambassador House to the south-east of the site Kettering and Laxton Court (both above the Tesco supermarket) to the east of the site. Moreover, the height of the building should act as an appropriate back-drop to the railway lines and in any case, views of the development from Brigstock Road would be significantly obscured by the existing built form fronting onto Brigstock Road and the surroundings roads. Therefore, the proposed massing and design would be acceptable in the context of the site and surroundings.

8.25 Whilst the proposal would have an overall contemporary appearance, the proposed elevations would reflect local character and create visual interest in its

shared pedestrian and vehicle access and it is likely that the access area, in conjunction with the proposed soft landscaped buffer planting, could serve as an additional community amenity and street-play area. Each of the individual towers would also be served by a landscaped communal roof terrace.

- 8.29 Overall, officers feel that the scheme is well considered and has strong potential (subject to a robust process taking place at planning conditions discharge stage) to become a successful addition to the urban fabric of the adjacent Thornton Heath District Centre and railway station – with positive benefits for the setting and ultimate (more intensive) use and adjacent Iceland supermarket as demonstrated in the submitted Illustrative Masterplan.
- 8.30 The site is not subject to any specific heritage designations. Whilst the nearest heritage asset to the site is Thornton Heath Clock-tower, its local view designation would not be affected by the proposed development.

Housing Quality for Future Occupiers

- 8.31 All of the proposed new units would comply with or exceed the internal dimensions required by the Nationally Described Space Standards (NDSS) with all units being dual aspect. All the ground floor flats would have private external amenity spaces in the form of private rear gardens that would meet minimum standards. All of the upper floor flats would have balconies that would also meet minimum standards. There would provision made for landscaped communal amenity roof terraces with raised planters and seating on each of the towers; the ground floor shared surface would provide a dual function, firstly allowing a turning head to be provided for delivery, refuse, servicing, and emergency service vehicles, and secondly allowing the area to be used as an informal recreation area and play street to provide children’s play space. The boundary fence enclosing ground floor amenity areas (ground floor flats facing out onto the railway) would be located 4 metres from the operational rail land. It is important that this boundary treatment not only provides privacy – but acts as a further sound barrier. Overall, the internal layout and arrangement of the proposed flats would make the best use of available floor space.
- 8.32 A noise impact assessment was submitted with the application. It concluded that no adverse effect would result to residential occupiers from the nearby railway lines/railway station if adequate mitigation is put into place (i.e. sound resistance measures such as double-glazing to attenuate noise). The actionable mitigation measures set out in the noise impact assessment can be secured by condition.
- 8.33 Conditions are recommended to protect future residents from undue noise and disturbance that could result from the proposed commercial uses. These would include:-
- Restrictions on the hours of operation of commercial uses.
 - Requirements for sound proofing between commercial and adjacent ground and first floor residences.

- 8.34 Each of the towers are proposed to be served by separate stair cores, each of which would have step free pedestrian access. In terms of accessibility, the London Plan sets requirements for 10% of homes to be designed to be wheelchair accessible or easily adaptable for residents who are wheelchair users. 6 dwellings (10%) have been designed as wheelchair accessible flats to meet M4(3) building regulation standards.
- 8.35 A daylight and sunlight analysis report was submitted with the application. It concludes the levels of daylight that would be achieved within the proposed habitable rooms in the new dwellings would satisfy the targets in the British Standard Code of Practice for Daylighting and Appendix C of the BRE Guidelines. It therefore demonstrates that the future occupants of those habitable rooms would have acceptable daylight amenity. Officers concur with the findings of the report.
- 8.36 Overall, the proposed development could provide an interesting and pleasant place to live for future occupiers – situated within a sustainable location, well connected to local amenities as well as public transport options. The proposed shared surface in front of the building would provide activity on the site and the commercial units could provide potential employment opportunities for some of the future occupiers. The shared surface and communal roof terraces could also provide places for residents to congregate and get to know one another. The site is conveniently located for Thornton Heath District Centre and as a consequence, has access to public transport and local shops and services. Effectively, a community within a community could result from the proposed scheme and this type of mixed use arrangement would be fully sustainable and should help reduce the need for travel.

Residential Amenity for Neighbours

- 8.37 Policy DM10.6 states that the Council will not support development proposals which would have adverse effects on the amenities of adjoining or nearby properties or have an unacceptable impact on the surrounding area. This can include a loss of privacy, daylight, sunlight, outlook or an increased sense of enclosure. The primary properties that would be affected by the proposed development are the adjacent dwellings to the south-west of the site on Melfort Road.

Daylight and Sunlight Effects

- 8.38 The daylight and sunlight analysis report submitted with the application concludes that although there would be several technical transgressions of the BRE Guidelines, none of the shortfalls would be materially below the BRE recommendations. The properties that front Melfort Road have a relatively open outlook to the north-east (looking out over the scaffold yard and the railway line beyond and already receive high levels of natural daylight. The worst case infringements would bring to the VSC levels below 27% threshold – which would mean that the change would be noticeable – although in view of the urban context and the general expectation that daylight would be infringed when larger building mass is promoted and proposed, reductions in daylight are to be expected. Even

with this, a 22% (worst case) VSC would still provide reasonable light levels for a urban typology. As the rear of Melfort Road properties face north of due east, sunlight impacts are not relevant. These numerical results therefore demonstrate that there would be no unreasonable impact on existing neighbouring amenity. Officers concur with the findings of the report.

Privacy Implications

- 8.39 The south-western flank elevation of the singular 6 storey tower block would be sited approximately 17.5 metres from the north-eastern rear elevations of the nearest houses on Melfort Road. The proposed north-west elevation to this proposed building would be generally imperforate and any side-on views from the recessed balconies would be restricted through use of privacy screens and landscaped features located on the roof top areas.
- 8.40 The south-western frontage of the 7, 8 and 9 storey towers would be sited between 22.94 metres and 25.94 metres from the north-eastern rear elevations of the nearest houses on Melfort Road.
- 8.41 Overall, there would be no adverse loss of privacy, outlook or daylight/sunlight as a consequence of this proposed development and the proposed distances between buildings would be suitably generous (for an urban setting).
- 8.42 With the recommended planning conditions, noise and disturbance associated with commercial uses should be suitably managed and should suitably protect future residents and existing residents living adjacent to the site. Whilst noise and general disturbance would result from demolition and construction works, a condition is recommended to ensure that a construction logistics plan is submitted for approval to manage and minimise disturbance.

Parking, Access and Highway Safety

- 8.43 The development would effectively be a 'car-free' development, albeit accommodating 2 car parking spaces allocated for disabled use. The Council would secure as a condition the provision of two electric vehicle charging points (one active and one passive) for these parking spaces. The Council would also seek provision of an on-site car club space with 3 years free membership for future residential occupiers. This matter could be secured as part of a S106 legal agreement and condition.
- 8.44 The proposed car-free development would accord with the London Plan standards that state all new developments in areas of good public transport accessibility should aim for significantly less than 1 space per dwelling. The site's close proximity to amenities and public transport would promote sustainable travel rather than single occupancy car use. Access to the adjacent railway yard would be protected and retained and arrangements would need to be satisfactorily agreed with Network Rail bearing in mind that development is proposed in close proximity to an operational railway.

- 8.45 A full transport assessment was submitted with the application. The number of trips expected to be generated by mode by the proposed scheme was calculated using the TRICS database and examining the National Traffic Survey. According to the TRICS assessment, the current site activity generates in the order of 16 daily vehicle trips, with 2 two-way trips in the AM peak and 0 trips in the PM peak. Of these 16 trips, there are also expected to be in the order of 3 other goods vehicle (OGV) trips to and from the site each day. As the residential element of the development is proposed to be car free, it has been assumed that all journeys made by residents will be via sustainable means. This is also likely to be the case for the office/commercial space although, for the sake of robustness, a TRICS Multimodal investigation was carried out to assess the likely impact of this element of the proposals. The trip generation analysis has assumed that the 2 disabled parking spaces are used overnight by residents and that these all depart for work in the AM peak and arrive back in the PM peak. The TRICS assessment shows it would generate 1 car movement for the commercial uses in the morning peak hour and 4 residential car movements split between AM and PM. Therefore, the transport assessment concluded that the number of vehicle trips would decrease as a result of the proposed development.
- 8.46 The forecast traffic flows and profiles of arrival and departure, plus the close proximity of the site to Thornton Heath Train Station and several bus routes on Brigstock Road and High Street should ensure that there would be no adverse material impact on the parking provision on the local road network. The site is adjacent to roads which are in a Controlled Parking Zone and the S106 Agreement makes provision to restrict the issue of resident parking permits to future residential occupiers (except disabled persons) of the development.
- 8.47 The development would be accessed via the existing access into the site, which measures approximately 5.7m wide at the entrance and leads directly onto the internal site access road that serves the Iceland store which itself is approximately 7m wide. The internal access is intended to be a shared surface with the main section of road measuring 4.1m in width, widening to an effective width of 6.1m when taking into account the footway that is flush with the carriageway. These dimensions would allow vehicles to pass one another without conflict. The proposed access would also utilise the existing bell-mouth entrance serving the site and adjacent Iceland store which is approximately 5.6m wide. This is an established access that would accommodate both vehicle movements associated with the Iceland store as well as those associated with the existing site including construction material delivery. The internal access road leading to the site is under the applicant's ownership, and it is proposed to be improved and upgraded to suit the future end users.
- 8.48 The transport assessment submitted with the application contains a swept path layout of a Fire Appliance that demonstrates that the vehicle can enter and leave in a forward direction, utilising the hammer-head at the northern end of the site for a three point turn. The fire appliance can therefore get within the required distance of the dwelling entrances.
- 8.49 A total of 100 cycle parking spaces for residents and 18 cycle parking spaces for visitors would be provided. Provision could also be secured by condition made

for charging for electric cycles and mobility scooters. The cycle storage provision would comply with the minimum standards set out in the London Plan.

- 8.50 A construction logistics plan and a delivery and servicing plan would be required for the proposed development. Full details can be secured by condition. In respect of the construction the proposal would also be subject to a financial contribution of £5,800 towards combating and monitoring air quality.
- 8.51 Transport for London were consulted on the application and had no objection to it subject the full constructions logistics plan and full delivery and servicing plan being secured by condition.
- 8.52 Residential refuse storage is proposed in two locations in the site. It is considered that refuse vehicles could enter and exit the site in a forward gear and turn safely, Full details of refuse vehicle tracking can be secured by condition together with a Refuse Management Plan. Officers are satisfied that these arrangements would operate effectively and in accordance with the Council's standard refuse collection and storage arrangements.

Flood Risk

- 8.53 The LLFA has no objection to the development subject to a condition requiring compliance with the submitted Flood Risk Assessment & Surface Water Drainage Strategy and further information being submitted for approval with regard to the detailed designs of a surface water drainage scheme.

Sustainability

- 8.54 Policy seeks high standards of design and construction in terms of sustainability and sets out Local and National CO2 reduction targets. An Energy Assessment/Sustainability Assessment has been provided, showing that whilst the minimum 35% on-site CO2 reductions beyond Part L of 2013 Building Regulations can be achieved (meeting local policy requirements through on-site energy efficiency measures and renewable technologies) zero carbon cannot be achieved on site. The remaining shortfall will therefore be offset through a cash-in-lieu contribution of £76,093 secured through the Section 106 Agreement.
- 8.55 Planning conditions are recommended to finalise the design as well as to demonstrate the CO2 and water use targets have been met following construction.
- 8.56 The residential part of the development proposes a range of sustainable design and construction features including:
- High performance building fabric and energy efficient lighting, services and equipment.
 - Passive design measures to reduce energy demand for heating, cooling, ventilation and lighting.
 - Combined Heat and Power, Photovoltaics and Air Source Heat Pumps.

- Water saving sanitary fittings and appliances to deliver a water efficient development.
- The use of materials with a low lifecycle environmental impact and embodied energy;
- Efficient construction and operational waste management.

8.57 The commercial uses will be expected to meet a BREEAM rating of “Excellent” – which will be secured through the use of a planning condition.

Trees, Landscaping and Biodiversity

8.58 There are no trees on the application site. There are several self-seeded trees on adjacent railway land to the north-east but they are not protected and it is considered they would not be compromised by the proposed development.

8.59 The indicative landscaping scheme shows soft and hard landscaping at ground floor and a shared surface which could be used as a play street for children. Each of the towers would also benefit from a fully landscaped roof terrace. Full details of hard and soft landscaping would be required (as reserved matters) as well as measures to promote biodiversity therein.

Other Matters

CIL

8.60 The development will be liable for a charge under the Community Infrastructure Levy (CIL). This payment will contribute to delivering infrastructure to support the development of the area, such as local schools.

Employment and Training Strategy

8.61 An employment and training strategy and contribution would be secured through a legal agreement to ensure the employment of local residents during construction and potentially at the end user phase.

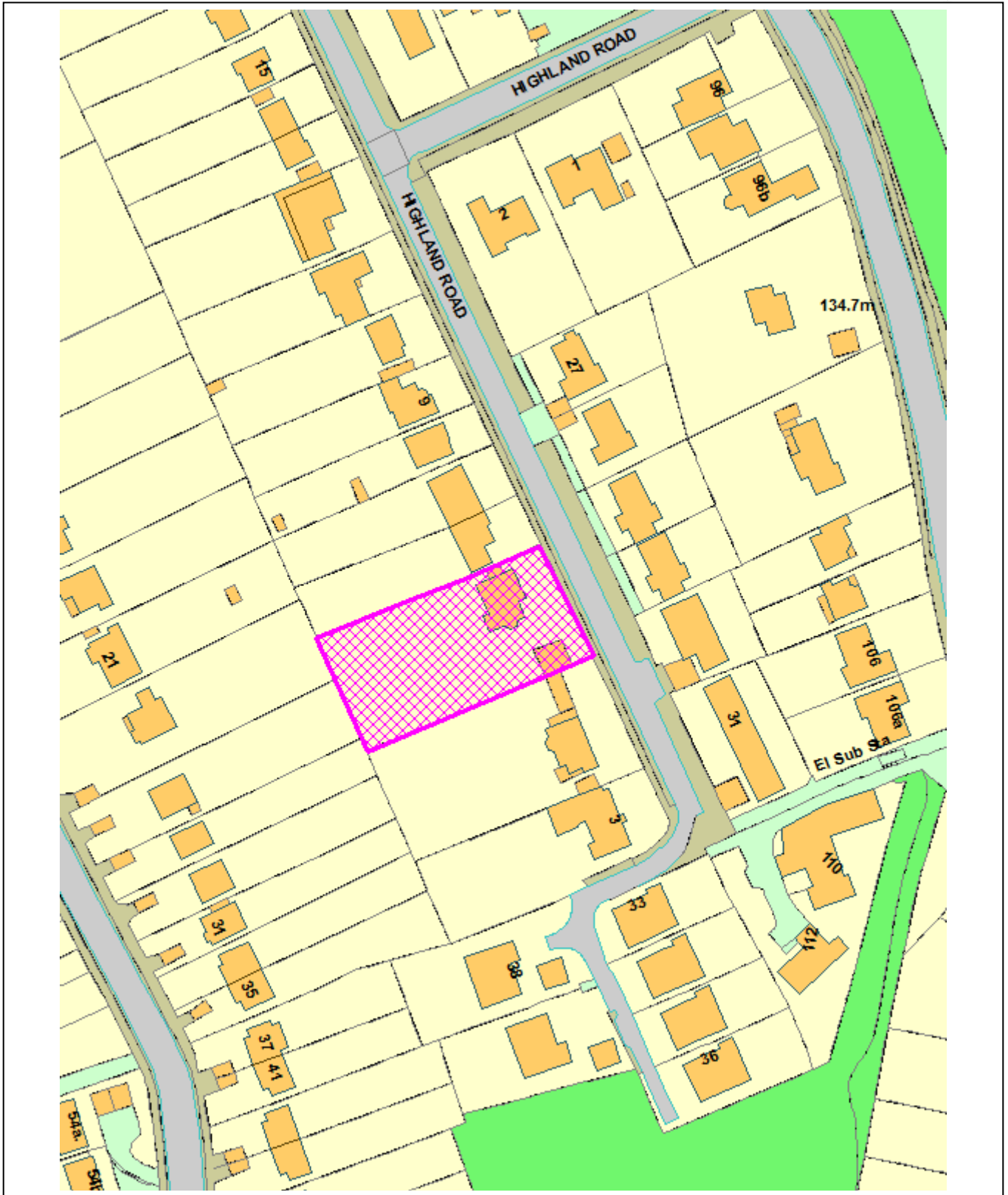
Conclusions

8.62 Given the Council promotes the re-provision of commercial uses on employment sites and given the significant need for housing within the Borough, and given the site is adjacent to a District Centre, then the principle of this mixed use development is considered acceptable within this area. The proposal provides affordable housing with both London Affordable Rent and Shared Ownership tenures being delivered.

8.63 The proposed design would respect the character and appearance of the area and would represent a sensitive and sustainable intensification of the site. Whilst it is acknowledged that the mass of built form would be greater than the existing structures of site, the proposal would be in context with the surrounding environment. The applicant has demonstrated through an Illustrative Masterplan that similar enhancements could be provided to the adjacent part of the Proposal

(i.e. Iceland and its car park). The proposal would have no significant harmful impact on the amenities of the adjacent properties and the application demonstrates that the impact on the highway network would be acceptable. Officers are therefore satisfied that the scheme is worthy of a planning permission.

8.64 All other relevant policies and considerations, including equalities, have been taken into account.



This page is intentionally left blank

1.0 APPLICATION DETAILS

Ref: 19/03074/FUL
 Location: 5 Highland Road, Purley, CR8 2HS
 Ward: Kenley
 Description: Demolition of existing detached house and detached garage and replacement with 9 new apartments in a single block of four / five storeys with parking to the front
 Drawing Nos: PL-050 Rev.20, PL PL-098 Rev.20, PL-099 Rev.20, PL-100 Rev.20, PL-101 Rev.20, PL-102 Rev.20, PL-200 Rev.20, PL201 Rev.19, PL202 Rev.19, PL-203 Rev.19, PL-300 Rev.19 Rev.19, PL-400 Rev.02
 Applicant: Macar Developments
 Agent: Paul Lewis
 Case Officer: Joe Sales

	1 bed	2 bed (3p)	2 bed (4p)	3 bed
Existing	-	-		1
Proposed flats	1 (1B2P)	1	4	3 (3B5P)

All units are proposed for private sale

Number of car parking spaces	Number of cycle parking spaces
8	18

1.1 This application is being reported to Planning Committee because the number of objections received in response to the planning application exceed the threshold in the Committee Consideration Criteria. Furthermore, the application was referred to planning committee by Councillor Steve O’Connell (Kenley Ward).

2.0 RECOMMENDATION

2.1 That the Committee resolve to GRANT planning permission subject to conditions and informatives to secure the following matters:

Conditions

1. Time limit of 3 years
2. Development to be carried out in accordance with the approved drawings and reports except where specified by conditions
3. Details of materials to be submitted

4. Hard and soft landscaping including boundary treatment and retaining walls to be submitted
5. Details of children's play-space to be provided
6. No additional windows in the flank elevations
7. Obscure glazing to windows in flank elevations at first and second floor if below 1.7m
8. Privacy screens to recessed balconies, details of which to be agreed
9. Car parking provided as specified.
10. Cycle storage
11. Highway restrictions
12. Details of electric vehicle charging point to be submitted
13. Construction Logistics Plan to be submitted
14. 19% Carbon reduction
15. 110 litre Water usage
16. Details of site specific SuDS to be submitted
17. Accord with recommendations of the Flood Risk Assessment
18. Accord with the Tree Protection Plan
19. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Community Infrastructure Levy
- 2) Code of practise for Construction Sites
- 3) Ecology consideration
- 4) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

2.3 That the Planning Committee confirms that adequate provision has been made by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

3.0 PROPOSAL AND LOCATION DETAILS

3.1 The proposal includes the following:

- Demolition of existing house
- Alterations to the land levels
- The erection of a four/five storey building with accommodation within the lower level and roof level to provide 9 flats, 3 of which would be 3 bedroom family units.
- Provision of communal external amenity space and children's play space
- Provision of 8 off-street parking spaces
- Provision of associated refuse and cycle stores

3.2 Amended drawings were received during the process of the application which were re-notified to the public. These included changes to the materials, front elevation and general arrangement.

Site and Surroundings

- 3.3 The site comprises a detached dwelling situated on the western side of Highland Road. Land levels fall from east to west (front of the site to the back). There is existing off street car parking, access and garage. The site has a low risk of surface water flooding and a Public Transport Accessibility Level (PTAL) of 0. The site is 0.8miles (a 19 minute walk) from Reedham Station and a 0.9miles (a 20 minute walk) from Kenley Station.

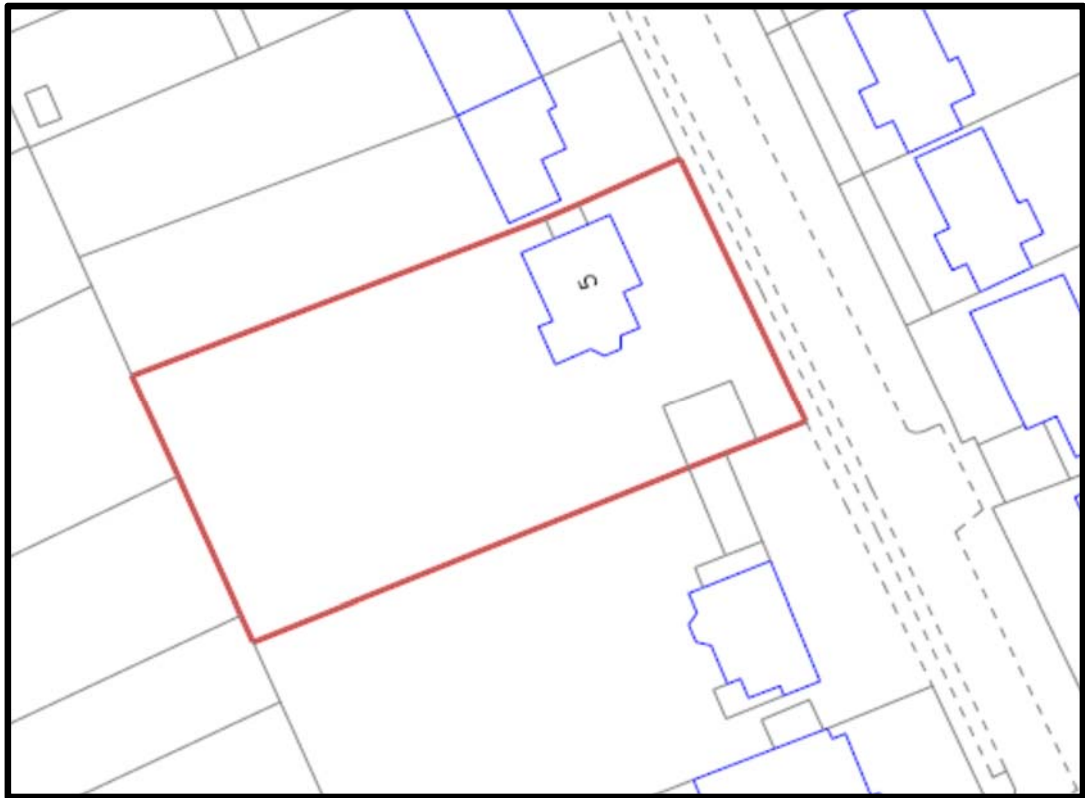


Fig 1: Site Location Plan.

- 3.4 Highland Road comprises a range of different types of houses in the form of detached two storey houses and bungalows. There is also a mix of various architectural styles along the street. Typical materials used for the external facades of the existing buildings are red brick and white render.

Planning History

- 3.4 00/01974/P-Planning permission was granted for the erection of a single storey side extension on the 27th of September 2000.
- 3.5 17/03034/FUL-Planning permission was granted at planning committee for the demolitions of an existing garage and the erection of a two storey detached building comprising a garage and office space/games room at first floor level to be used in conjunction with the main dwelling. 22nd of September 2017.

- 3.6 18/04109/PRE-The council engaged in pre-application discussions for the demolition of existing detached dwelling and replacement with nine new apartments.
- 3.7 19/00645/FUL-A full planning application was withdrawn on the 8th of May 2019 for the demolition of the existing detached house and detached garage and the erection of a three/four storey building with accommodation within the basement and roof level to provide 9 flats (comprising 6 x two bedroom and 3 x three bedroom units), access road to the side and the provision of 9 parking spaces to the rear, private and communal amenity space, play space, landscaping, bin store, cycle store, landscaping including land level alterations.

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of the development is acceptable.
- The design and appearance of the development is appropriate, respecting the character of the surrounding area.
- The living standards of future occupiers are satisfactory and Nationally Described Space Standard (NDSS) compliant.
- The living conditions of adjoining occupiers would be protected from undue harm subject to conditions.
- The level of parking and impact upon highway safety and efficiency is considered acceptable and can be controlled through conditions.
- Sustainability aspects can be controlled by conditions.

5.0 CONSULTATION RESPONSE

- 5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

- 6.1 The application has been publicised by 81 letters of notification to neighbouring properties in the vicinity of the application site. The number of representations received from neighbours in response to notification and publicity of the application are as follows:

No of individual responses: Objecting: 70 Supporting: 0 Neutral: 0
 No of petitions: 0

- 6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Objection	Officer comment
Design and appearance	
Overdevelopment of the site	Addressed in Townscape and Visual Impact.

Out of keeping with street	Addressed in Townscape and Visual Impact.
Impact on amenities of neighbouring properties	
Loss of light to neighbouring properties	Addressed in Residential Amenity for Neighbours.
Overlooking and loss of privacy for neighbours	Addressed in Residential Amenity for Neighbours.
Construction noise and dust will be harmful to local residents	Addressed in Access and Parking.
Landscape/Trees	
Detrimental impact on trees	Addressed in Trees and Landscaping.
Transport and Parking	
Inadequate parking provision.	Addressed in Access and Parking.
Negative impact on highway safety	Addressed in Access and Parking.

6.3 Councillor Steve O'Connell raised an objection to the planning application and referred it to planning committee based on the following considerations:

- Too dense
- Cramped development
- Insufficient parking
- Significant on road parking
- Access issues for emergency vehicles
- Safety

6.4 Councillor Jan Buttinger objected to the proposal on the following grounds:

- Insufficient parking
- Out of character due to bulk and massing
- Overlooking
- Safety issues during construction
- No lift or disabled parking
- Lack of landscaping

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application

and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Requiring good design.
- Promoting sustainable transport;
- Delivering a wide choice of high quality homes;

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture
- 7.21 Woodlands and trees

7.5 Croydon Local Plan 2018

- SP2 - Homes
- SP6.3 - Sustainable Design and Construction
- DM1 - Housing choice for sustainable communities
- SP4 – Urban Design and Local Character
- DM10 - Design and character
- DM13 - Refuse and recycling
- DM16 – Promoting healthy communities

- SP6 – Environment and Climate Change
- DM23 - Development and construction
- DM25 – Sustainable drainage systems and reducing floor risk
- SP7 – Green Grid
- DM27 – Biodiversity
- DM28 – Trees
- SP8 – Transport and communications
- DM29 - Promoting sustainable travel and reducing congestion
- DM30 - Car and cycle parking in new development

7.6 There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016
- Croydon Suburban Design Guide Supplementary Planning Document April 2019

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the Planning Committee are required to consider are as follows:

1. Principle of Development
2. Housing Quality for Future Occupiers
3. Townscape and Visual Impact
4. Residential Amenity for Neighbours
5. Accessibility and Parking
6. Sustainability and Environment
7. Trees and Landscaping
8. Other Matters

Principle of Development

8.2 The London Plan and Croydon Local Plan identify appropriate use of land as a material consideration to ensure that opportunities for development are recognised and housing supply optimised. It is acknowledged that windfall schemes which provide sensitive renewal and intensification of existing residential areas play an important role in meeting demand for new homes.

8.3 The application is for a flatted development providing 9 additional homes within the borough. The site is located within an existing residential area and the site is not allocated for any other purpose. Providing that the proposal respects the character and appearance of the surrounding area and there are no other impact issues, the principle of residential intensification is supported.

8.4 Croydon Local Plan Policy DM1.2 states that the Council will permit the redevelopment of residential units, where it does not result in the net loss of 3 bedroom homes (as originally built) or the loss of homes smaller than 130sqm. Policy SP2.7 supports the provision of new family sized dwellings, with a strategic target of 30% of all new dwellings across the borough to be family sized. 2 bed four person homes are also counted as family units for the first three year of the Local Plan subject to viability.

- 8.5 The existing building on site is a 3 bedroom house which would be replaced by two 3-bedroom units. There would be no net loss of three bedroom homes in line with Policy DM1.2. The proposed three-bedroom homes represent 22% of the overall new homes which would assist in meeting the 30% strategic target. Furthermore, 4 of the 2-bedroom homes would also be suitable for families, resulting in 6 or 75% of homes being suitable for 4 or more occupants.
- 8.6 London Plan Policy 3.4 states that development should optimise housing output for different types of location whilst taking account of local context and character. The site is a suburban setting with a PTAL of 0. As such the London Plan sets out an indicative density ranges of 150–200 habitable rooms per hectare (hr/ha). The London Plan also acknowledges that it is not appropriate to apply these ranges mechanically, and the density ranges are broad, to enable account to be taken of other factors relevant to optimising potential – such as local context, design and transport capacity.
- 8.7 The proposed density would be 205 hr/ha, which would optimise the site's housing output whilst respecting the local character in line with Policy 3.4.
- 8.8 The principle of the development can therefore be supported provided the proposal respects the character and appearance of the surrounding area and there are no other unacceptable impacts.

Townscape and Visual Impact

- 8.9 The road is characterised by large detached dwellings and bungalows which are predominantly one and two storeys in height and the existing architectural styles do vary throughout the street. The existing dwelling is an arts and crafts style dwelling however, it is not considered that the dwelling holds any significant architectural merit and therefore there is no objection to its demolition.
- 8.10 Policy DM10.1 states that proposals should achieve a minimum height of 3 storeys and the proposal is for a part four/five storey building with the fifth floor accommodated within the roof space (lit by gable/dormer features and roof-lights).
- 8.11 The Suburban Design Guide states that where surrounding dwellings are predominantly two storey detached dwellings, new development should seek to accommodate an additional storey within the roof space. As the lower ground level is set below the level of neighbouring properties, the proposed development would be one storey taller than the adjacent houses and would incorporate accommodation within the roof space. The proposed height would therefore be compliant with the Suburban Design Guide.
- 8.12 The new building would take on a “contemporary reinterpretation” form of the existing arts and crafts house which uses an extended gable to provide a defined arched entrance along with a feature window and feature brickwork which can be located on the side elevations of the building. The design of the building gives the appearance of a large detached dwelling with a front door. The building

would use a range of traditional materials such as smoke grey bricks to the lower level to the side and rear elevations, red bricks to the upper levels, grey upvc windows and grey slate effect roof tiles.



Figure 2: Elevation of proposed building as seen from street level

- 8.13 Given the topography of the land, to mitigate the dominant appearance of the rear elevation, grey bricks have been used on the lower ground floor and red bricks have been used for the upper floors. Furthermore the use of brickwork features such as soldier courses above and below the window reveals and the recessed balconies throughout the proposed building ensure that there is not an overall dominant appearance when viewing the building from the rear or neighbouring gardens.
- 8.14 Whilst the building would have a greater footprint than the existing house, given the layout of surrounding buildings and that the proposed building will be set down from the existing highway, the impact on the street-scene would be acceptable. The separation distance between the proposed and existing buildings will be around 6m and 14m respectively. The separation between the proposed and neighbouring buildings ensures that the rhythm of the street scene is retained.
- 8.15 The Suburban Design Guide states that Croydon's topography presents many opportunities for new development in semi-submerged lower floors with level access on one side of a property. In other settings, it may be possible to provide fully submerged basements or lower-ground floor development, however these

are often considered to be uncharacteristic of suburban settings and need to be carefully designed to minimise any negative impacts on the streetscene.

- 8.16 A sloping topography can provide opportunities to work with the landscape to achieve greater footprints which extend beyond neighbouring elevations by stepping the building mass. By stepping built form down a slope, impacts on neighbours can be avoided. It is important that the rhythm of stepping follows the gradient of the slope to avoid large built form protruding from the hillside.
- 8.17 Basements, lower-ground floor development and massing that steps down a slope that do not require the introduction of light wells will generally be acceptable provided that any habitable rooms have sufficient access to natural light. The proposed building does not require any light wells to provide sufficient light to the units located within the lower ground floor. Furthermore, the building has optimised the use of land levels on this site and the lower ground floor units have been designed so that the proposed occupiers will benefit from direct access to amenity space which is located on the existing garden area.
- 8.18 The depth of the proposed built form would be greater compared to the existing dwelling at 5 Highland Road, however the scheme would comply with the 45 degree guidance as detailed by the Suburban Design Guide (both vertically and on plan) and is therefore considered that the impact on the amenity and outlook of the neighbouring properties will be acceptable.
- 8.19 Access driveways, forecourt parking and retaining walls to properties are features commonly found on Highland Road. The existing access and driveway will be replaced and raised with 8 parking bays which would form a forecourt that is accessed directly off the existing highway.



Figure 3: Proposed Site Plan

- 8.19 The proposed parking forecourt would be softened through the use of landscaping which would be located around the front boundary of the site and between the parking area and proposed building. Given that the parking areas and residential amenity space/existing highway is separated by landscaping, the hardstanding and retaining walls would not have an overly dominant or incongruous impact on the visual amenities of the area or the street scene.
- 8.20 The applicant has explored various avenues with regards to the provision of an internal bin store. Given that this is not feasible on this scheme, an external bin store has been provided at the front of the site. The bins will be screened by a wood and brick structure which will not cause undue harm to the existing street scene and landscaping can be provided around the store.
- 8.21 The proposal would overall result in a development that would respect the pattern and rhythm of the neighbouring area given the design of the building which is a modern interpretation of an arts and crafts style building. The style, design and appearance of the dwelling will not harm the appearance of the street scene.

8.22 Therefore, having considered all of the above, against the backdrop of housing need, officers are of the opinion that the proposed development would comply with the objectives of the above policies in terms of respecting local character.

Housing Quality for Future Occupiers

8.23 All of the proposed new units would comply with internal dimensions required by the Nationally Described Space Standards (NDSS).

8.24 With regard to external amenity space, the London Housing SPG states that a minimum of 5sqm of private outdoor space should be provided for 1-2 person dwellings and an extra 1sqm for each additional unit. All of the units are provided sufficient amenity space through balconies which is in accordance with the London Housing SPG.

8.25 A communal garden is provided at the rear of the site which was the garden of the original dwelling. As all of the units have private amenity space, the provision of supplementary communal amenity space is considered acceptable. A child play space is shown to be provided within the communal garden, details of which can be secured by condition.

8.26 London Plan Policy 3.8 and the London Housing SPG together promote accessible design, whilst advocating a flexible approach on small scale developments. The Housing SPG clarifies that Policy 3.8 should be applied flexibly to ensure that residential or mixed use development is deliverable and notes that a lift may cause practical difficulties for small scale developments (where the depth and width of a plot and height restrictions may inhibit the overall floorplate and massing).

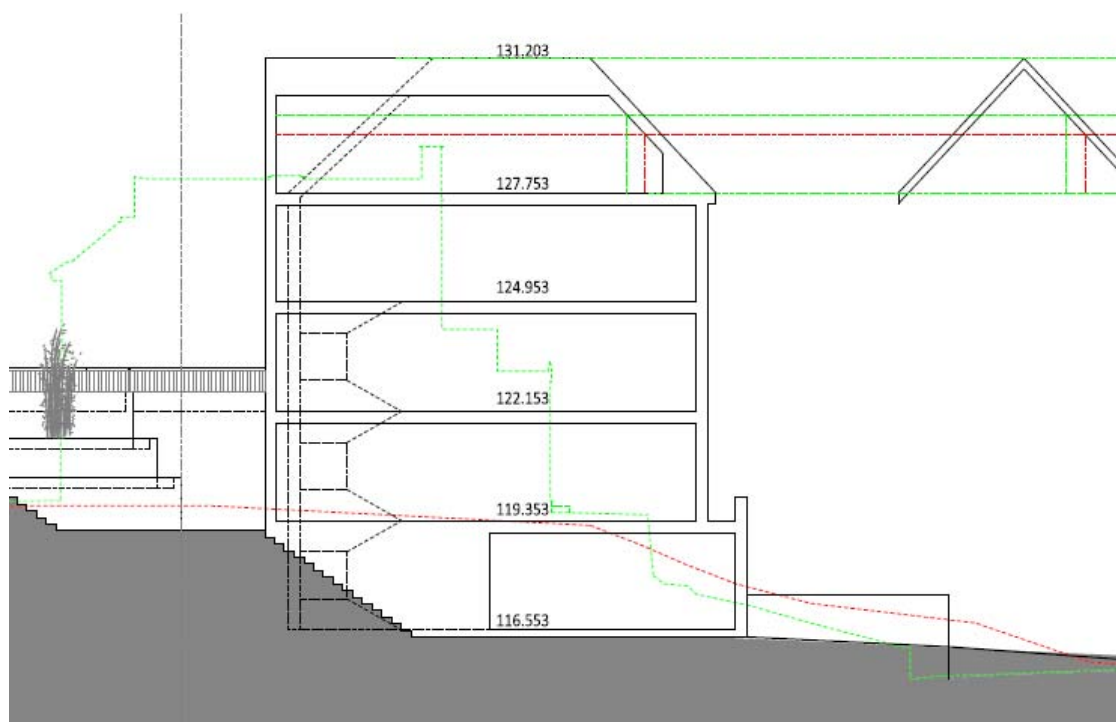
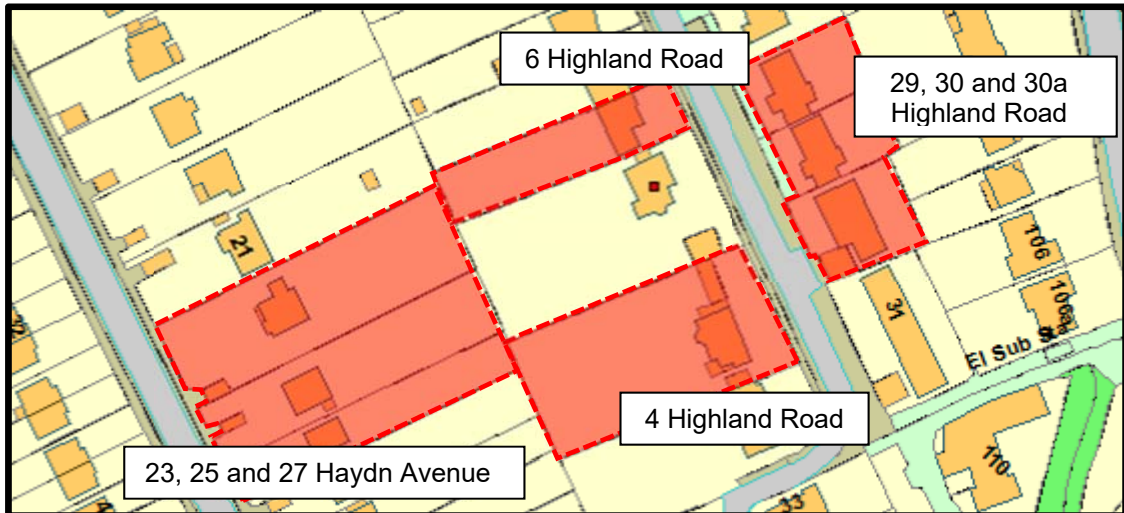


Figure 4: Proposed Section

- 8.27 In this case, there is step free access to the building from the highway, despite the slope of the street. The ground floor level of the building is level with the highway and there is adequate space to the front of the site to provide level access to serve the building. Whilst it is noted that there is not level access to the communal amenity space located within the rear garden, the units located on the upper ground floor provide step free access onto the private direct amenity spaces. Therefore, whilst M4(2) standard cannot be fully complied with the upper ground floor units would comply internally.
- 8.28 A lift could not reasonably or practically be provided within the building whilst also delivering the number of proposed units, due to the limited floorspace. However, all units have a WC on the same floor as the living room, and step free access to outdoor amenity spaces.
- 8.29 Given that the building is set down due to the topography of the site a daylight assessment was carried out by the applicant to assess the daylight which enters. Whilst it is noted that the assessment relates to the previous scheme as shown within the appendices the layout of the building has not altered and therefore the conclusions of the report are still relevant and reliable.
- 8.30 The conclusions of this report show that the internal average daylight for bedrooms 1 and 2 of apartments 2 and 3 achieve and exceed the minimum requirements in accordance with the BRE guidelines.
- 8.31 Given the site's particular circumstances (its topography, limited size, and infill position); the requirement to respect the character of the street scene; and the steps taken to ensure a proportionate degree of accessible design, the development is considered acceptable under the circumstances.
- 8.32 The development is considered to result in a high quality development including 3 x three bedroom family units all with substantial amenities and overall providing an acceptable standard of accommodation for future occupiers.

Residential Amenity for Neighbours

- 8.33 The main properties that would be affected by the proposed development are as follows:
- 4 Highland Road and 6 Highland Road
 - 29, 30 and 30a Highland Road
 - 23, 25 and 27 Haydn Avenue



4 and 6 Highland Road

8.34 These are the existing neighbouring properties located to either side of the proposed development boundary. Whilst the proposal would be set back and deeper than the plots at 4 and 6 Highland Road the lower ground floor would be set down into the ground. When assessed against the Suburban Design Guide, the proposed rear elevation wall would not encroach into the “rule of thumb” 45 degree angle, taken from the rear windows of 4 and 5 Highland Road (either horizontally or vertically). Therefore the proposal would not be unduly overbearing or cause an unacceptable loss of outlook for both of the neighbouring properties.

8.35 There are no windows proposed that would cause any loss of privacy to the neighbouring property, with proposed side facing ground floor windows either facing the boundary fence or being secondary in function (installed at high level) and obscure glazed below 1.7 metres from the internal floor level.

8.36 The rear balconies would all be recessed and screened to the side and as such, should limit overlooking sideways and into the rear gardens to 4 and 6 Highland Road. The impact to these properties is acceptable.

29, 30 and 30a Highland Road

8.37 These are the detached dwellings which are located on the opposite side of the road. Whilst the proposed development would be substantially larger than the existing dwelling, given the distance between these properties and that they are set higher due to the topography of the road, the level of overlooking from the front of the proposed development across the street would be limited and therefore the impact on the amenity and outlook of the occupiers of these dwellings is minimal.

23, 25 and 27 Haydn Avenue

8.38 These residential properties are located to the rear of the application site with all of the rear gardens being around 35 metres in depth. The rear wall of the

proposed development would be sited a minimum depth of 25 metres from the rear boundary and given the depth of the neighbouring gardens, the window to window separation would be excessive at around 50 metres. The scheme would therefore maintain adequate light, outlook or privacy. There are existing mature trees and hedges along the rear boundary which also increase screening between the proposed development and the existing houses on Haydn Avenue and therefore the loss out of outlook and amenity to these properties would be minimal.

Access and Parking

- 8.39 This site has a PTAL of 0 which indicates poor public transport accessibility. The Council's Transport Planner has assessed the submitted details and advised that the development would be expected to generate up to a maximum of 11 parked cars based on the London Plan recommends that 3-bed units should provide a maximum of 1.5 spaces per unit and smaller units should provide less than one space.
- 8.40 8 parking spaces are proposed on site. A parking stress survey which has been submitted as part of the application demonstrates that there are at least 27 spaces to accommodate potential overspill parking within close proximity to the site. There is capacity to accommodate the anticipated overspill of up to 3 vehicles on street.
- 8.41 Whilst the Suburban Design Guide states that in areas of PTAL 0-1, developments should seek to accommodate all parking within the site, the developer has provided the maximum number of parking spaces possible within the site whilst respecting the existing building line and it is considered that the overspill of up to 4 vehicles on street will not have a detrimental impact on highway safety.
- 8.42 Local Plan Policy DM30 states that 20% of parking bays should have Electric Vehicle Charging Points (EVCP) with future provision available for the other bays. Details and provision of the EVCP will be subject to a condition.
- 8.43 Concern has also been expressed by neighbours with regards to construction vehicles accessing the site. It is noted that other large vehicles for deliveries and the like have access to Highland Road as existing. Prior to any works taking place on site, a Construction Logistics and Management Plan will be required to be submitted to the local planning authority by condition to ensure that the road situation has been taken into account. This plan will also detail hours of deliveries, and limit construction hours to reduce the impact on the adjacent properties as far as possible.
- 8.44 A cycle storage area is provided on the lower ground floor, accommodating 18 cycle spaces. The cycle storage is accessed for the external staircase to the side of the proposed building. A cycle grove would be installed and a condition is proposed to confirm further details of this. 17 cycle parking spaces would need to be provided in line with London Plan requirements (1 space for 1 bed flats and 2 spaces for all other units). Full details of the proposed storage

method demonstrating that the space is large enough for 18 cycles will be secured by condition.

- 8.45 The proposed refuse storage is located on the forecourt of the proposed building. It would be adjacent to the highway which is convenient and suitable for refuse collectors and is well screened.
- 4.46 Full details of the bin store have been submitted and are acceptable. The access and storage to the cycle and refuse store is secured and in order to prevent any anti-social behaviour from occurring; full details of these arrangement will be secured by conditions.

Environment and Sustainability

- 8.47 Conditions are recommended to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.
- 8.48 The site is located within an area of low risk of surface water flooding. A Flood Risk Assessment has been submitted as part of the application recommending flood resistance and drainage measures. The proposals seek to infiltrate surface water into the ground in the first instance and therefore surface water will be discharged to a soakaway located on the road situated along the front of the site. The new parking areas along the front of the property will utilise permeable paving in order to provide surface water treatment.
- 8.49 Policy DM25 requires all development to incorporate sustainable drainage measures (SuDS) and the FRA outlines that SuDS measures should be installed. A condition requiring site specific SuDS measures would be imposed on any planning permission, alongside the other recommendations of the FRA.

Trees, Landscaping and Biodiversity

- 8.50 The site is covered by a Tree Preservation Order, 25 (2015) which sits on the boundary between 5 and 6 Highland Road. A full Arboricultural Assessment has been submitted in support of the application.
- 8.51 None of the trees to be removed, a total of 5, are protected by a tree preservation order or considered worthy of special protection. The site offers ample opportunity for the planting of replacement and additional new trees as part of a landscaping scheme. Details of this will be secured by condition.
- 8.52 Following a review of the proposals and the full Arboricultural Assessment, no objections have been raised in relation to the impact on the trees subject to the protection measures being implemented as set within the Arboricultural Assessment and the submission of a full landscaping plan. The proposals are

therefore acceptable from an arboricultural perspective and comply with policy DM28 of the Croydon Local Plan.

- 8.53 Objections have been made in relation to the existing biodiversity that has been located on the site in recent times. Local residents have provided evidence which shows deer foraging within the gardens of the site in question and the neighbouring properties. Deer are not protected species. It is considered that the proposed communal amenity space will still provide a sufficient area for species to use as they have done previously once the construction works of the development has been completed and a condition can secure biodiverse planting.

Other matters

- 8.54 The development will be liable for a charge under the Community Infrastructure Levy (CIL). This payment will contribute to delivering infrastructure to support the development of the area, such as local schools.

Conclusions

- 8.55 The principle of development is considered acceptable within this area. The development has been designed to ensure its appearance respects the character of the surrounding area and that there is no significant adverse impacts on neighbouring occupiers. The impact on the highway network would be acceptable with adequate car parking being made available. Therefore, the proposal is considered to be in accordance with the relevant policies.
- 8.55 All other relevant policies and considerations, including equalities, have been taken into account.

This page is intentionally left blank



This page is intentionally left blank

PART 6: Planning Applications for Decision

Item 6.4

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/03385/FUL
 Location: 34 Caterham Drive, Coulsdon, CR5 1JF
 Ward: Old Coulsdon
 Description: Demolition of existing dwelling and garage and erection of a three storey building comprising 6 flats (3 x 3 bed, 1 x 2 bed and 2 x 1 bed), 3 parking spaces, communal amenity space and cycle / refuse / recycle storage
 Drawing Nos: 570.10 Rev A; 570.11 Rev A; 570.12 Rev F; 570.13 Rev E; 570.14 Rev F; 570.15 Rev F; 570.16 Rev F; 570.17 Rev C; CGI's received 16/10/2019 and 31/10/219; Flood Risk Assessment; Conceptual Surface Water strategy report dated 01/08/2019; External Daylight Study; Residents Engagement Report dated 09/08/2019; Parking Stress Survey & Analysis dated July 2019.
 Applicant: Mr Joseph Garner & Ben Fish of New Place Associates
 Case Officer: Lucas Zoricak

	1B 2P	2B 3P	2B 4P	3B 4P	3B 5P	Total
Existing Provision				1		1
Proposed Provision	2		1	1	2	6

1.1 This application is being reported to Planning Committee because objections above the threshold in the Committee Consideration Criteria have been received.

2.0 RECOMMENDATION

2.1 That the Committee resolve to GRANT planning permission subject to conditions and informatives to secure the following matters:

Conditions

1. Development to be carried out in accordance with the approved drawings and reports except where specified by conditions
2. Details of facing materials
3. Obscure glazed window openings
4. Hard and soft landscaping to be submitted
5. Construction Logistics Plan
6. Details of refuse and cycle storage, boundary treatments
7. Permeable forecourt material
8. Drainage - Details in accordance with Surface Water Strategy Report
9. Sustainable urban drainage details

10. M4(2) compliance
11. Details of the play space
12. 19% reduction in CO2 Emissions
13. 110l Water Restriction
14. Details of parking spaces
15. Reinstatement of the existing vehicular crossover in Haycroft Close
16. Construction of a new vehicular crossover in Catherham Drive
17. Parking restrictions
18. Time limit of 3 years
19. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) CIL
- 2) Party Wall Act 1996
- 3) Code of practise for Construction Sites
- 4) Highways – site and highway boundary details, surface water discharge
- 5) Highways – costs associated with highway works
- 6) Demolition guidance
- 7) Demolition notice
- 8) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

3.0 PROPOSAL AND LOCATION DETAILS

3.1 The proposal includes the following:

- Demolition of existing detached 3 bed house
- Erection of a three storey building comprising 3 x 3 bedroom flat (1 x 4 person and 2 x 5 person), 1 x 2 bedroom flat (4 person) and 2 x 1 bedroom flat (2 person).
- Provision of 3 off-street spaces.
- Provision of associated refuse/cycle stores.

3.2 Amended plans were received showing an amended design/appearance, material, fenestration, access, internal layout and parking layout following consultation response from strategic transport and urban design officers. No reconsultation was needed because the amendments did not lead to a material change in circumstances.

Site and Surroundings

3.3 The application site lies on the southern side of Caterham Drive on the junction with Haycroft Close. The site is currently occupied by a single storey detached dwelling that is positioned towards the rear boundary. There is a detached garage located to the rear of the house that is accessed off Haycroft Close. The site benefits from a generous front garden with soft landscaping, whereas the rear garden is more limited by the existing garage. The ground level rises along Haycroft Close.

3.4 The surrounding area is residential in character and is comprised of a variety of dwelling types and sizes. There are no site specific constraints that would impact upon the development potential of the site and neither is the site subject to a formal tree preservation order. The site has a Public Transport Accessibility Level (PTAL) of 1A; therefore it is considered to have poor access to public transport. The application site is located within an Archaeological Priority Area (Tier III) and lies within an area at medium/high risk of surface water flooding as identified by the Croydon flood maps. There are no other constraints affecting the application site as identified by the Croydon Plan.



Fig 1: Aerial street view highlighting the proposed site within the surrounding street-scene

Planning History

- 3.5 19/01557/PRE - Proposed demolition of existing dwelling and garage: Erection of a three storey building with accommodation in the roof space to provide 7 flats (comprising 7 x two), 4 parking spaces, private amenity space, bin storage facilities and landscaping including retaining walls.
- 3.6 18/03090/FUL - Demolition of existing dwelling and garage; proposed erection of 2no. two-storey three bedroom semi-detached dwellings, formation of vehicular access and provision of associated parking – Approved - 27.09.2018.

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The proposed development would create good quality residential accommodation that would make a positive contribution to the borough's housing stock and would make a small contribution to the Council achieving its housing targets as set out in the London Plan (2016) and Croydon Local Plan (2018). The proposed development would provide an appropriate mix of units including 3x three-bed units, 1x two-bed unit and 2x one-bed unit.

- The proposed development would be of an appropriate mass, scale, form and design that would respect the established built form of the surrounding area.
- The proposed development would not cause unacceptable harm to the amenities of neighbouring residential occupiers.
- Subject to the imposition of conditions, the proposed development would not have an adverse impact on the operation of the highway.
- Subject to conditions, the proposals would not have an adverse impact on flooding.
- Sustainability aspects can be controlled by conditions

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

6.1 The application has been publicised by 16 letters of notification to neighbouring properties in the vicinity of the application site. A site notice has been displayed in Haycroft Close and in Caterham Drive on 16.08.2019. The number of representations received from neighbours, a Residents' Association (Old Coulsdon Residents Association and Caterham Flood Action Group) and a local ward Councillor in response to notification and publicity of the application are as follows:

No of individual responses: 246 Objecting: 246 Supporting: 0 Comment: 0

6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Summary of objections	Response
<i>Principle of development</i>	
Poor quality development	Addressed in the report at paragraphs 8.2 – 8.14
Overdevelopment and intensification	Addressed in the report at paragraphs 8.2 – 8.6
An unwelcome precedent	Addressed in the report at paragraphs 8.2 – 8.6
Loss of family home	Addressed in the report at paragraph 8.4 – 8.5
<i>Design</i>	
Out of character	Addressed in the report at paragraphs 8.7 – 8.14
Excessive scale, height and massing	Addressed in the report at paragraphs 8.7 – 8.14
Over intensification – Too dense	Addressed in the report at paragraphs 8.7 – 8.14
Visual impact on the street scene (out of character)	Addressed in the report at paragraphs 8.7 – 8.14

Harm to the suburban character	Addressed in the report at paragraphs 8.7 – 8.14
Number of storeys	Addressed in the report at paragraphs 8.7 – 8.14
<i>Amenities</i>	
Negative impact on neighbouring amenities	Addressed in the report at paragraphs 8.15 – 8.20
Loss of light	Addressed in the report at paragraphs 8.15 – 8.20
Loss of privacy	Addressed in the report at paragraphs 8.15 – 8.20
Overlooking	Addressed in the report at paragraphs 8.15 – 8.20
Disturbance (noise, light, pollution etc.)	Addressed in the report at paragraphs 8.20
<i>Traffic & Parking</i>	
Negative impact on parking and traffic in the area	Addressed in the report at paragraphs 8.26 – 8.34
Inadequate provision of off-street parking	Addressed in the report at paragraphs 8.27 – 8.30
Negative impact on highway safety	Addressed in the report at paragraph 8.27 – 8.34
Refuse and recycling provision	Addressed in the report at paragraph 8.33
<i>Other matters</i>	
Impact on / loss of existing trees	Addressed in the report at paragraphs 8.35
Impact on wildlife and biodiversity	Addressed in the report at paragraphs 8.36
Impact on surface flooding	Addressed in the report at paragraph 8.38
Construction disturbance	Addressed in the report at paragraph 8.39
Increased pressure on local infrastructure and services	Addressed in the report at paragraph 8.41

6.3 Cllr Margaret Bird (Old Coulsdon Ward) has referred the application to committee and raised the following issues:

- Overdevelopment of the site
- Visual dominance on the streetscene
- Increased surface flooding risk
- Loss of amenity to neighbours
- Out of character
- Inadequate parking provision
- Negative impact on the wildlife (badgers)

6.4 Old Coulsdon Residents Association and Caterham Flood Action Group have raised the following issues:

- The new development would not mitigate the flood risk
- Maintenance of the flat sedum roof has not been clarified
- Increased surface flooding risk
- No provision of flood resilience to existing homeowners
- Unsympathetic design
- Out of character with local context
- Overbearing height and massing
- The building line not being respected
- Dominant appearance
- Increased surface water flooding risk
- Insufficient parking provision
- Inadequate provision of amenity space

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations. Such determination shall be made in accordance with the Plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan (February 2018), and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF) revised in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivery of housing
- Promoting social, recreational and cultural facilities and services the community needs
- Requiring good design.

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2016

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 5.1 Climate change mitigation

- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.7 Renewable energy
- 5.10 Urban greening
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.14 Water quality and wastewater infrastructure
- 5.15 Water use and supplies
- 5.16 Waste net self sufficiency
- 5.18 Construction, Demolition and excavation waste
- 6.3 Effects of development on transport capacity
- 6.9 Cycling
- 6.10 Walking
- 6.11 Smoothing traffic flow and tackling congestion
- 6.12 Road Network Capacity
- 6.13 Parking
- 7.4 Local Character
- 7.6 Architecture
- 8.3 Community infrastructure levy

7.5 Croydon Local Plan (adopted February 2018)

- SP1 – The places of Croydon
- SP2 – Homes
- DM1 – Housing choice for sustainable communities
- SP4 – Urban Design and Local Character
- DM10 – Design and character
- DM13 – Refuse and recycling
- SP6 – Environment and Climate Change
- DM23 – Development and construction
- DM24 – Land contamination
- DM25 – Sustainable drainage systems and reducing flood risk
- SP7 – Green Grid
- DM27 – Biodiversity
- DM28 – Trees
- SP8 – Transport and Communications
- DM29 – Promoting sustainable travel and reducing congestion
- DM30 – Car and cycle parking in new development
- DM37 – Coulsdon

7.6 Suburban Design Guide Supplementary Planning Document (SPD) 2019

The SPD is a Housing Design Guide that provides guidance on suburban residential developments and extensions and alterations to existing homes across the borough. The SPD is a design guide for suburban developments likely to occur on windfall sites where existing homes are to be redeveloped to provide for several homes or proposals for building homes in rear gardens.

7.7 Other relevant Supplementary Planning Guidance as follows:

- London Housing SPG, March 2016
- National Technical Housing Standards, 2015
- National Planning Practice Guidance

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The principal issues of this particular application relate to:

- The principle of the development;
- Impact of the development on the character and appearance of the area;
- Impact on residential amenities;
- Standard of accommodation;
- Transport and highways impacts;
- Impacts on trees and wildlife;
- Sustainability issues; and
- Other matters

The Principle of Development

- 8.2 Both the London Plan and the NPPF place significant weight on housing delivery and focus on the roles that intensification and small sites in particular can play in resolving the current housing crisis. It is acknowledged that windfall schemes which provide sensitive renewal and intensification of existing residential areas play an important role in meeting the demand for additional housing in Greater London, helping to address overcrowding and affordability issues.
- 8.3 The site is a windfall site which could be suitable for sensitive renewal and intensification. The residential character of Caterham Drive consists of detached and semi-detached houses.
- 8.4 Local Plan Policy DM1.2 seeks to prevent the loss of small family homes by restricting the net loss of 3 bed units and the loss of units that have a floor area less than 130 sq.m. The existing property has a floor area of 100.3 sq.m and is a 3 bed bungalow; however, on the basis that this would be replaced with 4 family size units, which would result in a net gain of family accommodation, this is considered acceptable.
- 8.5 The proposed 3 x 3 bed and 1 x 2 bed 4 person units would provide adequate floorspace for four families. Policy SP2.7 sets a strategic target of 30% of new homes to be 3-bedroom homes and CLP acknowledges that 2 bed, 4 person homes can be treated as family homes (in line with DM1.1) during the first 3 years of the Plan. The overall mix of accommodation would be acceptable and would result in a net gain in family accommodation.
- 8.6 Representations have raised concern over the intensification of the site and overdevelopment. The site is in a suburban setting with a PTAL rating of 1a and as such, the London Plan indicates that a suitable density level range is between 150-200 habitable rooms per hectare (hr/ha). Whilst the proposal would be in excess of this range (337 hr/ha), it is important to note that the London Plan indicates that it is not appropriate to apply these ranges mechanistically, and also

provides sufficient flexibility for higher density schemes (beyond the density range) to be supported where they are acceptable in all other regards. In this instance the proposal is acceptable, respecting the character and appearance of the surrounding area, and does not demonstrate signs of overdevelopment (such as poor quality residential units or unreasonable harm to neighbouring amenity). As such the principle of the scheme is supported.

The effect of the proposal on the character of the area and visual amenities of the street-scene

- 8.7 The existing property with a detached garage is not protected from demolition by existing policies and its demolition is acceptable subject to a suitably designed replacement building coming forward. The proposal seeks to replace it with 6 units within a single building fronting Caterham Drive.
- 8.8 The surrounding area includes a mixture of one storey and two storey dwellings. The existing dwelling at No. 36 is a detached bungalow whereas the property on the other side of the site at No. 30 is a two storey detached house. The property positioned to the rear of the application site is a two-storey detached dwelling house. Single storey bungalows can be seen in the street scene adjacent to two storey houses.
- 8.9 The Croydon Local Plan has a presumption in favour of three storey development, where respecting the character of the area, and the application seeks to provide a three storey property providing a high quality built form that respects the land level changes, pattern, layout and siting in accordance with Policy DM10.1.
- 8.10 Whilst it is acknowledged that the proposed dwellings would be taller than the neighbouring properties, this relationship is considered acceptable and in line with the SDG (2019) which advises that where surrounding buildings are predominantly detached dwellings of two or more storeys, new developments may be three storeys with an additional floor contained within the roof space or set back from the building envelope below, as a contemporary reinterpretation of the existing built form. Furthermore, the proposed building would be suitably set back off the main road so as to not appear overly prominent, and the land is at a higher level to the rear. Given the building line varies along the street, the position of the building is considered acceptable. Separation distances of approximately 2m have been incorporated to either side boundaries ensuring the development does not appear overly cramped in relation to its plot. The third floor would be set back and would help to break up the bulk and massing of the building.



PROPOSED FRONT ELEVATION (Caterham Drive)

Fig 2: Elevational view highlighting the proposal in relation to neighbouring properties.



PROPOSED SIDE ELEVATION (Haycroft Close)

Fig 3: Elevational view highlighting that the massing respects Haycroft Close properties.

8.11 It is also acknowledged that the proposed scheme with contemporary design and roof form would not replicate the predominant local character, but rather be a contemporary reinterpretation of it, as advocated by the SDG (2019). The scheme does however respect existing character for example by having a similar width to those generally found in the area, similar floor to ceiling heights and a recessed upper floor, as well as incorporating materials (brickwork) predominant in the area. The NPPF states that *innovation, originality or initiative through unsubstantiated requirements to conform to certain development forms or styles should not be stifled*. The proposal would not have a detrimental impact on the existing character, which itself is inconsistent and varied, with a mix of post-war built forms. The site presents a compelling opportunity to pursue a contemporary

approach, as unique additions to such an area may positively evolve its character.



Figure 4 CGI of site showing the front elevation fronting Caterham Drive

8.12 The frontage includes hard-standing to allow for off street parking. There would be some soft landscaping surrounding it, along with a section of soft landscaping along the front/front-side boundary. The existing vehicle access off Haycroft Close would be reinstated and a new vehicular crossover would be formed to the front of the property with access from Caterham Drive. Sustainable urban drainage systems (SUDs) would be used at the site and conditioned as part of the approval. Furthermore a number of properties benefit from front driveways and this has become an established element of the Caterham Drive streetcene. The proposed forecourt with a driveway and parking provision would therefore reflect the arrangement of the neighbouring buildings and is considered acceptable.



Fig 5: Ground floor plan proposed site showing proposal in relation to neighbouring properties

- 8.13 The application site is a substantial plot within an established residential area and is comparable in size to other flatted developments approved throughout the borough. The scale and massing of the new build would generally be in keeping with the overall scale of development found in the immediate area and the layout of the development would respect the pattern and rhythm.
- 8.14 In the light of the above, officers are of the opinion that the proposed development would comply with the objectives of the above policies and the Suburban Design Guide SPD (2019) in terms of the visual amenity.

The effect of the proposal upon the amenities of the occupiers of adjoining properties

- 8.15 Policy DM10.6 states that the Council will not support development proposals which would have adverse effects on the amenities of adjoining or nearby properties, or have an unacceptable impact on the surrounding area. This can include a loss of privacy, a loss of natural light, a loss of outlook or the creation of a sense of enclosure. The properties with the potential to be most affected are the adjoining properties at 36 Caterham Drive, 30 Caterham Drive, 1 Haycroft Close and the dwellings opposite on Catherham Drive.

36 Caterham Drive

- 8.16 This dwelling is orientated to the south of the application site and is set at approximately the same level as the proposal site. No.36 has been extended with a single storey front garage adjacent to the original dwelling and flank wall facing the application site. Both the property and the garage are set back approximately 2 metres from the (side) boundary with the application site and the separation distance between the flank wall of the proposed building and flank wall of this neighbouring property would range between 2.4 and 3 metres. While it is noted that the footprint of the proposed building would project approximately 6 metres beyond the front building line of No.36, the outlook and light levels to front ground floor openings have already been affected by the presence of a 7 metres deep garage with a hipped roof. The dormer window positioned near the boundary with the application site serves a bathroom, the other dormer window serves a habitable room and the proposed site plan (570.12 Rev F) indicates the 45 degree rule compliance in relation to the proposed building in plan form. Some side facing windows are proposed above ground floor. Where these face on to the roof slope of no 36 Caterham Drive these are clear glazed as they do not give rise to significant overlooking. At second floor level the side facing windows are proposed as obscure glazed as they serve a bathroom and a living area which would be able to look out over the dormer of no 36 Caterham Drive. By ensuring these are obscure glazed there would be no significant loss of privacy to 36. Given the siting, orientation, presence of the garage and separation distance, the proposed building is not considered to have a detrimental impact on the amenity of these adjoining occupiers through the loss of light, outlook, privacy, overlooking, overbearing form or sense of enclosure.



Fig 5: CGI of site showing the side elevation with main entrance fronting Haycroft Close

30 Caterham Drive

8.17 This property is located on the opposite side of Haycroft Close from the development. This, and the distance of 9 metres between the side elevation of the proposed building and the side elevation of this two-storey property is considered sufficient to prevent any harmful impact on the amenities of these occupiers. In addition, this property does not have any side habitable room windows facing the application site. As such the garden space of 30 Caterham Drive is not considered to be directly overlooked and so the proposal complies with the SDG.

1 Haycroft Close

8.18 This dwelling is orientated to the west of the application site and is set at an elevated position in relation to the application site. It is noted that the property benefits from a side dormer window serving habitable rooms and ground floor side facing habitable rooms. It should also be noted that the SDG makes clear that minimal protection should be given to side facing windows. There would be 13.5m from the proposed rear elevation of the building and the side elevation of no.1 Haycroft Close. The ground floor side facing windows of no 1 Haycroft Close are already affected by their fence and the proposal would not result in no sky being visible from that property, a significant element of the BRE daylight and sunlight tests and the development would be located to the north east of these windows, resulting in light and outlook still being visible from the side. The dormer window would be generally unaffected as it is on a similar level to the recessed upper floor of the proposed development. Therefore, the scheme would be unlikely to cause a loss of natural light, a loss of outlook or the creation of a sense of enclosure. Furthermore, the proposed rear first and second floor openings

would be obscure glazed and would therefore not give rise to overlooking / privacy issues.



Fig 6: CGI of site showing the side and rear elevation

Nos.47, 49, 51 and 53 Caterham Drive (opposite)

8.19 It is considered that given the separation distances and topography that there would not be a significant impact on these dwellings in terms of loss of light, outlook, privacy or sense of overbearing. This is considered to be an acceptable relationship in a suburban setting such as this.

8.20 With regard to the noise and disturbance, the proposed development would not result in undue noise, light or air pollution as a result of an increased number of occupants on the site. The increased number of units would increase the number of vehicle movements to and from the site, but this would not be significant and would not be overly harmful. As such, the proposed development is considered acceptable in terms of the neighbouring amenity.

The effect of the proposal upon the amenities of future occupiers

8.21 The Nationally Described Space Standards (NDSS) 2015 provide minimum technical space standards for new dwellings in terms of the gross internal floor areas and storage. All of the proposed units would meet the minimum required gross internal floor area.

8.22 The proposed floor space for each unit would meet the minimum requirements of the Nationally Described Space Standards for units of this type. Whilst some windows are obscure glazed to ameliorate any impact on neighbouring properties, all habitable rooms have a clear glazed window proposed. For units 4 & 6 this is provided by the semi-enclosed amenity space. Given the orientation

of the site sufficient light and outlook would be provided. The unit's would have access to private and communal amenity space which meets the required standard.

- 8.23 The local plan also requires all flatted development to provide new child play space on top of the amenity space to be provided. In terms of the child play space, this is indicated on the drawing 570.12 Rev F and further details would be secured through use of planning conditions.
- 8.24 In terms of accessibility, the scheme achieves M4(2) compliance. Specifically, the ground units would have step free access from the front door and to the refuse store, and reasonable level access to the communal space to the rear. Whilst the inclusion of a lift shaft would provide level access to units at upper floors, it would have had significant impact on viability and would have certainly reduced the number of rooms and/or flats and would have reduced the capacity of the site to optimise the number of units sustainably achievable. In such circumstances, the London Plan advises that units above or below the ground floor should satisfy M4(1), which is achievable. Whilst this flexibility does not apply to ground floor units, in view of the particular site circumstances and in order to protect the character of the street scene against the backdrop of housing need, the proposal is considered acceptable without providing level access to the upper floors.
- 8.25 Overall, the development is considered to result in a high quality development, including an uplift in family accommodation, and would offer future occupiers a good standard of amenity, including the provision of communal amenity space and child play space, and thus accords with relevant policy.

The impact on transport / highway

- 8.26 The Public Transport Accessibility Level (PTAL) rating is 1a which indicates poor accessibility to public transport. The London Plan and Policy DM30 of CLP (2018) sets out that maximum car parking standards for residential developments based on public transport accessibility levels and local character. This states that 1-2 bedroom properties should provide a maximum of up to 1 space per unit, with up to 1.5 spaces per unit being provided for 3 bedroom properties. In line with the London Plan, the proposed development could therefore provide up to a maximum of 7.5 spaces. It is important to note however that it is not necessarily desirable to provide car parking up to the maximum standards given the requirements of both the London Plan and Croydon Local Plan which seek to reduce reliance on car usage and promote/prioritise sustainable modes of transport. As such, a lower level of car parking can be supported and is encouraged in line with the ambitions of the Development Plan.
- 8.27 The site is not located in a controlled parking zone. Three on-site parking bays are proposed, in-line with draft London Plan guidance. However, while the low PTAL is acknowledged and a greater parking demand is likely, the proposed scheme would provide 50 per cent parking provision. The results of an on-street parking survey also suggests there is significant spare on-street parking capacity (with an average of 47 spaces being readily available) to accommodate the

development and unrestricted parking space along the surrounding highway network could accommodate an overspill of 5 vehicles on-street close to the site.

- 8.28 On the basis that parking overspill can be anticipated, Local Planning Authority would seek contributions to fund single/double yellow parking restrictions on the corners of the junction at Caterham Drive and Haycroft Close and on the approach to the bus stop on Caterham Drive.
- 8.29 There are a number of representations that refer to the parking provision, on-street parking and highway safety at the site. In respect to highway safety, the scheme provides 3 off-street parking spaces and these would need to adhere to the parking visibility splays and parking standards to ensure that safety requirements are adhered to and these have been secured through conditions.
- 8.30 The parking layout and number of parking space have been amended and the number of spaces has been reduced from 4 to 3. This was necessary because the bay number 4 closest to the development was unworkable as the vehicle would not be able to enter and exit in forward gear due to the balcony/amenity space of the ground floor jutting into the car park.
- 8.31 The formation of a new vehicle crossover and reinstatement of the existing crossover in Haycroft Close have been conditioned.
- 8.32 In compliance with the London Plan, electric vehicle charging points should be installed in the parking area and this shall be secured by way of a condition. Cycle storage facilities would comply with the London Plan (which would require 12 spaces) as these are located in the communal hallway.
- 8.33 The refuse arrangements would be acceptable and for a six units scheme would provide 4 x 240ltr landfill; 4 x 240ltr for dry recycling and 1 x 140ltr food recycling, which has been accommodated within the site. The refuse store would be located to the side of the building facing Haycroft Close with access to the hardstanding. Further details regarding elevational treatment and external material have been conditioned.
- 8.34 While a Construction Logistic Plan has been submitted with the application, the document has not been considered satisfactory and a submission of a revised Demolition / Construction Logistic Plan (including a Construction Management Plan) would be required before commencement of work, this could be secured through a condition. Subject to conditions, S106 and S278 agreements, the proposal is considered acceptable on transport and highway grounds.

Impact on trees and wildlife

- 8.35 While it is noted that the site benefits from a number of trees and soft landscaping, the site is not covered by a Tree Preservation Order, nor is the site within a Conservation Area so trees on the site are not subject to planning controls. The proposed plans indicate the retention of two trees and the removal of one tree and a hedge. A detailed landscaping plan has not been submitted and the submission of further hard and soft landscaping details has therefore

been conditioned. The proposed replacement planting locations are considered acceptable subject to submission of these details.

- 8.36 In terms of wildlife and biodiversity, the site is not in a protected area and there is insufficient evidence especially given the characteristics of the site (residential property with garden) to suggest that there is protected flora and fauna on site. The scheme includes a green roof, contributing to cooling the building, providing green space for wildlife and some water attenuation.

Sustainability Issues

- 8.37 Conditions can be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.

Other Matters

- 8.38 The site is located in Flood Zone 1 (land assessed as having a less than 1 in 1,000 annual probability of river or sea flooding (<0.1%)) as identified by the Environmental Agency and lies within an area at medium/high risk of surface water flooding. The applicant has carried out infiltration testing at the site as part of a detailed Surface Water Strategy Report (dated September 2018). The recommendations within this report would be conditioned as part of any approval at the site. The permeable parking area has been conditioned and would provide capacity for surface water runoff from hardstanding areas. The submission of a SuDS has also been conditioned.
- 8.39 Representations have raised concern that construction works would be disruptive and large vehicles could cause congestion and damage to the highway. Whilst the details submitted to date might not be fully acceptable, it would be prudent to condition a Construction Logistics Plan to be approved, as appointed contractors may have an alternative approach to construction methods and the condition ensures that the LPA maintains control to ensure the development progresses in an acceptable manner.
- 8.40 The application site is located within an Archaeological Priority Area (Tier III). Historic England has been consulted and raised no concerns. As such, the archaeological area, by reason of the proposed siting of the development, would not be affected.
- 8.41 Representations have raised concerns that local schools and other services would be unable to cope with additional residents in the area. The development would be liable for a charge under the Community Infrastructure Levy (CIL). This payment would contribute to delivering infrastructure to support the development of the area, such as local schools.

Conclusions

- 8.42 Having considered all of the above, against the backdrop of housing need, officers are satisfied that the proposed development would comply with the objectives of the above policies, subject to the provision of suitable conditions.
- 8.43 All other relevant policies and considerations, including equalities, have been taken into account.

This page is intentionally left blank



This page is intentionally left blank

PART 6: Planning Applications for Decision

Item 6.5

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/03410/FUL
 Location: 22 Purley Knoll, Purley, CR8 3AE
 Ward: Purley and Woodcote
 Description: Demolition of existing buildings; Erection of 7no. apartments and 2no. dwellings and associated works
 Drawing Nos: 18221LS; 18221E; 799/018/P01/A; 799/018/P02/A; 799/018/P03/D; 799/018/P04/C; 799/018/P05/B; 799/018/P06/A; 799/018/P07/C; 799/018/P08; 799/018/P09/B; 799/018/P10/B; 799/018/P11/A; 799/018/P12/A; 799/018/P13; 0226/19/B/1B
 Applicant: Everett Property Consultants
 Agent: Neal Thompson
 Case Officer: Chris Stacey

	1B2P	2B3P	2B4P	3B4P	TOTAL
TOTAL	2	1	3	3	9

All units are for private sale

Car parking spaces	Cycle parking spaces
4 (Inc. 1 blue badge)	16

1.1 This application is being reported to committee because objections above the threshold in the Committee Consideration Criteria have been received.

2.0 RECOMMENDATION

2.1 That the Planning Committee resolve to GRANT planning permission subject to:

A. The prior completion of a legal agreement to secure the following planning obligations:

a) Restriction on future residents obtaining car-parking permits

2.2 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. Time limit of 3 years
2. Implemented in accordance with approved drawings
3. Submission of a construction management plan
4. Further details of sustainable drainage measures
5. Materials and detailed drawings to be submitted

6. Landscaping details to be submitted (including child play space, boundary treatments, external lighting and maintenance measures)
7. Details of widened vehicular access and visibility splays
8. Details of cycle parking and cycle ramp
9. Provision of blue badge space and electric vehicle charging points
10. Refuse store to be provided prior to first occupation
11. Ground floor units to be Part M4(2) / Part M4(3) compliant
12. Obscure glazing and non-openable windows below 1.7m (where necessary)
13. 19% carbon reduction
14. 110litre water usage
15. In accordance with details and recommendations of arboricultural report, preliminary ecology report and bat survey report
16. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

1. Community Infrastructure Levy
2. Code of practice for construction sites
3. S.278 (highways) works
4. Wildlife protection
5. Refuse collection arrangements
6. Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

3.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of intensifying the residential use of the existing site to provide a greater quantum of homes than existing is acceptable.
- The design and appearance of the development is acceptable. Subservient buildings are proposed to the rear to optimise the development potential of the site.
- The living standards for future occupiers would be acceptable and Nationally Described Space Standard (NDSS) compliant.
- The living conditions of adjoining occupiers would be protected from undue harm subject to conditions.
- The level of parking and impact upon the local transport network is considered acceptable subject to conditions.
- The proposal's impact on trees and biodiversity is acceptable subject to conditions. Sustainability aspects can be controlled by conditions.
- The proposed flooding and sustainable drainage measures are acceptable subject to conditions.

4.0 SITE LOCATION AND PROPOSAL DETAILS

Site and Surroundings

- 4.1 The application site measures 16m in width and 65m in depth, has an overall area of 0.1ha, and is currently occupied by a detached 3 bedroom two storey property. The site features a garden to its front alongside a driveway leading to a standalone garage which is accessed via a dropped kerb. A large rear garden sits to the rear of the existing property and features a notable drop in land level towards its rear, with the end of the rear garden sitting circa 2.5m lower than the front of the site.



Figure 1: Location of site

- 4.2 The site sits on the north-east side of Purley Knoll, a two-way residential side street terminated by Woodcote Valley Road to the west and Brighton Road (A23) to the east, and sits approximately 200m west of the boundary of Purley District Centre. The surrounding area is principally residential in character, featuring a variety of detached properties on large plots arranged in a similar fashion to the application site, with some modest sized flatted blocks located nearby on Woodcote Valley Road. The site does not fall within a conservation area, with the nearest statutory listed building to the site being the Grade II listed Purley Library on Banstead Road.

Proposal

- 4.3 The application seeks to demolish the existing two storey property and erect a four storey (inclusive of a lower ground storey at its rear) building at the front of the site accommodating seven homes as well as a pair of two storey semi-detached houses at the rear.
- The four storey building at the front would accommodate 2 x 1 bedroom, 4 x 2 bedroom and 1 x 3 bedroom apartments.
 - The pair of two storey semi-detached houses at the rear would both feature 3 bedrooms.
 - 4 parking spaces would be located to the front of the site, accessed via a replacement crossover (located in the same position as the existing vehicle access).
 - A communal garden with child play space is proposed to the rear of the site.
 - Refuse storage is located within the building envelope (accessed via the side access route) with cycle storage located to the rear of the flatted building.

Planning History

- 4.4 The following planning decisions are relevant to the application:

22 Purley Knoll (Application Site):

17/00876/HSE – Erection of front garage and two storey side extension.

Permission granted May 2017, not implemented.

1 Woodcote Valley Road:

18/02493/FUL – Demolition of the existing house: Erection of a two storey building with accommodation within the roof space comprising of 2x three bedroom, 5x two bedroom and 2x one bedroom flats: Provision of associated parking and landscaping.

Permission granted July 2018, implemented.

3 & 5 Woodcote Valley Road:

19/04349/FUL – Demolition of two existing properties and erection of 3/4 storey building with accommodation in the roof space to provide 22 apartments, including landscaping, communal/child play space, enlarged vehicular access and parking as well cycle and refuse storage.

Currently under consideration.

6-12 Woodcote Valley Road:

17/05209/FUL – Demolition of existing buildings: erection of two/three storey building with accommodation in roofspace comprising 26 Retirement Living apartments for older persons including communal facilities: provision of vehicular access and provision of car parking and associated landscaping.

Permission refused April 2018, appeal allowed March 2019, implemented.

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

6.1 A total of 9 neighbouring properties were notified about the application and invited to comment. The number of representations received from neighbours, local groups etc. in response to notification and publicity of the application were as follows:

No of individual responses: 23 Objecting: 23 Supporting: 0

No of petitions received: 0

6.2 The following issues were raised in representations that are material to the determination of the application, and they are addressed in substance in the next section of this report:

SUMMARY OF OBJECTIONS	RESPONSE
Principle of development	
Overdevelopment / in excess of density range	The proposed density falls within the recommended range for this location

No affordable housing proposed	There is no requirement to provide affordable housing for the scale of development proposed
Loss of a family home without replacement family accommodation	The proposed development would result in a net uplift in family accommodation
Design	
Out of character with surrounding area	Please refer to paragraphs 8.5 – 8.15 of this report
Inadequate access arrangements for the two semi-detached properties to the rear	Please refer to paragraphs 8.5 – 8.8 and 8.19 of this report
Height and massing is unacceptable	Please refer to paragraphs 8.9 – 8.11 of this report
Poor quality of proposed accommodation	Please refer to paragraphs 8.16 – 8.19 of this report
Insufficient amenity space proposed for future occupants	Please refer to paragraphs 8.16 – 8.19 of this report
The architectural style of the proposal should better reflect that of the existing surrounding buildings	Please refer to paragraphs 8.12 – 8.15 of this report
Amenity	
Negative impact on neighbouring amenity	Please refer to paragraphs 8.20 – 8.28 of this report
Overlooking	Please refer to paragraphs 8.20 – 8.28 of this report
Increase in noise levels	The proposal is for a C3 (residential) use and would therefore not introduce noise levels which would be incompatible with other C3 (residential) uses
Disturbance from construction	A condition is recommended requiring the submission of a construction management plan prior to the commencement of works
Highways & Parking	
Insufficient car parking	Please refer to paragraphs 8.29 – 8.34 of this report
Parking survey is inadequate	The submitted parking survey has been undertaken in accordance with the 'Lambeth Methodology'
Negative impact on highway safety and local environment due to increased traffic levels	Please refer to paragraphs 8.29 – 8.34 of this report
Poor access to cycle storage	Please refer to paragraph 8.33 of this report
Other matters	
Impact upon local infrastructure	The proposed development would be CIL liable which assists in delivering infrastructure in the local area
Loss of natural vegetation and natural habitat	Please refer to paragraphs 8.35 – 8.40 of this report

Detrimental impact on trees	Please refer to paragraphs 8.35 – 8.36 of this report
No's 22 and 24 are physically connected at ground floor	This is noted and any issues arising from this would be addressed via the Party Wall Act 1996
Concerns regarding the ongoing maintenance of the site	A condition is recommended requiring the submission of a maintenance plan for the ongoing maintenance of the site
Impact on Purley View nursing home	The Purley View nursing home sits approximately 175m to the east of the site and is not considered to be adversely impacted upon by the proposed development
Smells from refuse store	The location of the proposed refuse store has been amended such that it is located within the building envelope, thus reducing the likelihood of smells from the refuse store impacting upon surrounding residents

6.3 Councillors Oviri and Quadir made the following representations:

- Overdevelopment
- Out of character with surrounding area
- Negative impact on neighbouring amenity
- Negative impact on highway safety and local environment due to increased traffic levels
- Increase in noise levels
- Inadequate access arrangements for the two semi-detached properties to the rear
- Poor quality of proposed accommodation
- Insufficient car parking
- Poor access to cycle storage

6.4 Purley and Woodcote Residents Association made the following representations:

- Loss of a family home without replacement family accommodation
- Overdevelopment
- Out of character with surrounding area
- Insufficient amenity space proposed for future occupants
- Poor quality of proposed accommodation
- Negative impact on neighbouring amenity
- Insufficient car parking
- Inadequate access arrangements for the two semi-detached properties to the rear

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any

other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise.

National Guidance

7.2 Government guidance is contained in the National Planning Policy Framework (**NPPF**) revised in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up to date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Delivering a sufficient supply of homes;
- Promoting healthy and safe communities;
- Promoting sustainable transport;
- Making effective use of land;
- Achieving well-designed places.

Development Plan

7.3 The Development Plan comprises the Croydon Local Plan 2018 (**CLP**), the London Plan 2016 (**LP**), and the South London Waste Plan 2012 (**SLWP**).

7.4 A replacement for the LP, in the form of the Draft London Plan 2019 (**DLP**) has been subject to public consultation and an examination in public (EiP). Whilst the LP still forms part of the adopted Development Plan, as the DLP moves further through the process to adoption it gains more weight. At present members should be advised that the DLP only carries limited weight.

7.5 Of particular relevance to this proposal is the Council's Suburban Design Guide 2019 (**SDG**) which is a supplementary planning document (SPD) intended to provide supplementary guidance to the CLP.

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the committee must consider are:

1. Principle of Development
2. Design and Appearance
3. Housing Quality
4. Impact on Surrounding Neighbours
5. Highways, Parking and Refuse
6. Trees, Landscaping, Biodiversity and Sustainability
7. Flood Risk and Sustainable Drainage

Principle of Development

8.2 The site's existing use is residential and as such the proposed redevelopment of the site for residential purposes is acceptable. Policy SP2.2 of the CLP states that the Council will seek to deliver a minimum of 32,890 homes between 2016 and 2036, with 10,060 of said homes being delivered across the borough on windfall sites (i.e. non allocated sites outside of the Croydon Opportunity Area – such as this application site).

Given the above the principle of intensifying the residential use of the existing site to provide a greater quantum of homes than existing is acceptable.

- 8.3 With respect to density, the site's 'setting' can be classed as 'urban' (despite its built form being of a suburban character) due to its close proximity to Purley district centre, and given its PTAL of 5 a suitable density level range is between 200-700 habitable rooms per hectare (hr/ha) in accordance with Table 3.2 of the LP. The proposal has a density of 300 hr/ha which is within the recommended range and is thus acceptable.
- 8.4 The existing property which is to be demolished is a 3 bedroom property (as originally built) measuring 160sqm and can be classified as a family home. Policy DM1.2 of the CLP seeks to protect family homes through not allowing the net loss of 3 bedroom homes (as originally built) and Policy SP2.7 of the CLP sets a strategic target for 30% of all new homes over the plan period to have 3 or more bedrooms (where demand is greatest) which all developments are expected to contribute towards. In this instance the proposed development would provide 3 x 3 bedroom homes (representing 33% of the homes proposed and a net uplift of 2 x 3 bedroom homes on this site) which would meet the requirements of both of the aforementioned policies. It should also be noted that Policy DM1.1(b) of the CLP allows 2 bedroom 4 person homes to be considered as family accommodation within 3 years of the adoption of this plan, and when the 3 x 2 bedroom 4 person homes are also taken into account, the development would feature a family home provision of 66% which is welcomed.

Design and Appearance

Layout

- 8.5 The proposed layout of the development includes the provision of a flatted block at the front of the site located in a similar location to the existing property (albeit with a larger footprint) and a pair of semi-detached properties located to the rear of the site, accessed via a footpath to the side of the flatted block. Car parking is provided at the front of the site, whilst a communal garden and child play area is located between the rear of the flatted block and the front of the semi-detached properties.
- 8.6 In respect of the flatted block at the front of the site, whilst the front building line sits further forward than the existing building and the neighbouring property at 20 Purley Knoll, it would still sit slightly back from the predominant front building line along the street and would thus be compliant with the guidance regarding building lines set out in the CLP and SDG. The rear projecting portion of the building does not project substantially beyond the rear building line of the existing property, extending beyond the rear building line of 20 and 24 Purley Knoll by 5.7m and 4.5m respectively (respecting the 45 degree angle from the nearest windows of the adjacent properties). A gap of 1.8m and 1.2m between each side of the building and the respective boundaries to 20 and 24 Purley Knoll has been proposed according with the relevant guidance regarding relationships to neighbouring boundaries in the SDG. Access to the flatted block is via a communal entrance located in the centre of the front façade which provides direct access to both the car parking area and street. Direct access from the flatted block to the communal garden and child play area is provided via a pathway located on the north-west side of the building adjacent to the boundary with 24 Purley Knoll.
- 8.7 In respect of the semi-detached properties located to the rear of the site, whilst it is recognised that this aspect of the proposal would represent a deviation from the

existing building pattern within the vicinity of the site, the relevant sections of the SDG regarding development in rear gardens support the principle of positioning development in rear gardens where they are subservient in scale to the existing surrounding built form, minimise their impact upon neighbouring amenity, and do not prejudice similar development from coming forward on neighbouring sites in the future. In this instance the proposed semi-detached properties would be subservient in scale to both the flatted building to the front of the site and the existing surrounding properties, have been set in from both side boundaries by 3m and are 8.5m from the rear boundary, and are arranged in a fashion which would not preclude a similar form of development from coming forward on neighbouring sites in the future and are thus acceptable. Access to the semi-detached properties is via a stepped pathway (featuring a gate with fob access) located on the south-east side of the building adjacent to the boundary with 20 Purley Knoll which is 1.8m in width (having been widened during the course of the application), which would provide an acceptable dedicated and secure access route to the semi-detached properties at the rear.

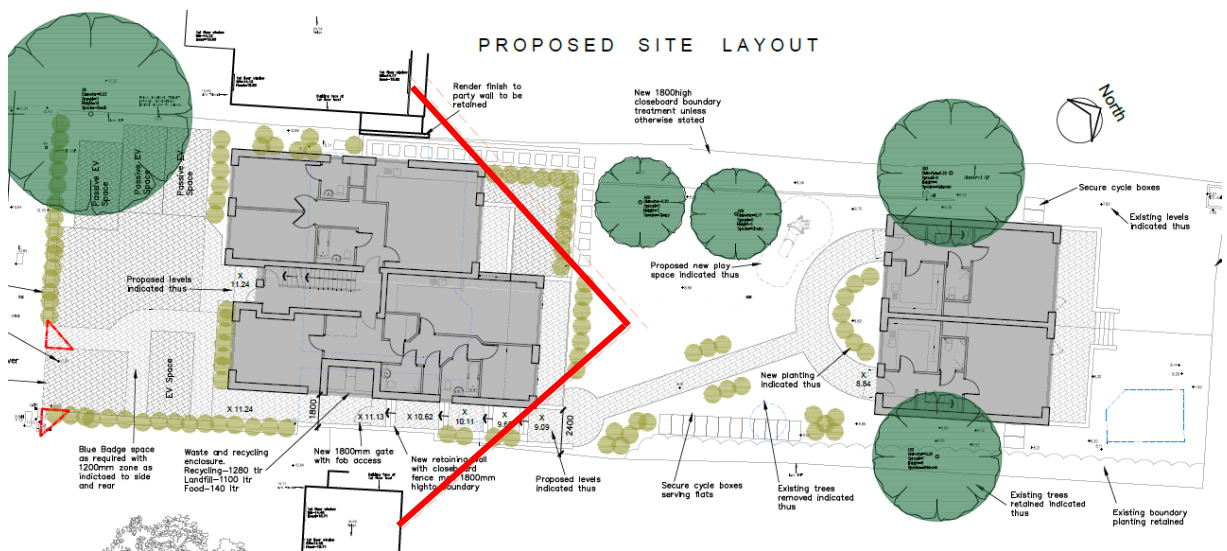


Figure 2: Proposed site plan

- 8.8 The front of the site will feature an area of hardstanding accommodating car parking for the proposed development, accessed via a replacement crossover (located in the same position as the existing vehicle access). Planting along the front boundary and the south-east boundary (with 20 Purley Knoll), as well as to the front of the flatted block, is proposed to soften the overall appearance of the front of the site, with the existing Beech tree (adjacent to 24 Purley Knoll) proposed to be retained. The area between the flatted block to the front of the site and the semi-detached houses to the rear of the site will be predominantly soft landscaped and will house the communal amenity and child play space provision for the site, along with 12 secure cycle boxes for the apartments within the flatted block to the front of the site.

Scale, Height and Massing

- 8.9 Policy DM10.1 of the CLP requires proposals to respect the scale, height and massing of the surrounding area, whilst seeking to achieve a minimum height of 3 storeys. Section 2.10 of the SDG provides further guidance for suburban residential developments (such as this proposal) stating that where surrounding buildings are

predominantly detached dwellings of 2 or more storeys, new developments may be 3 storeys with an additional floor contained within the roof space or set back from the building envelope below. Furthermore the SDG also states that developments do not necessarily need to step down in height towards neighbouring buildings of a lesser height.

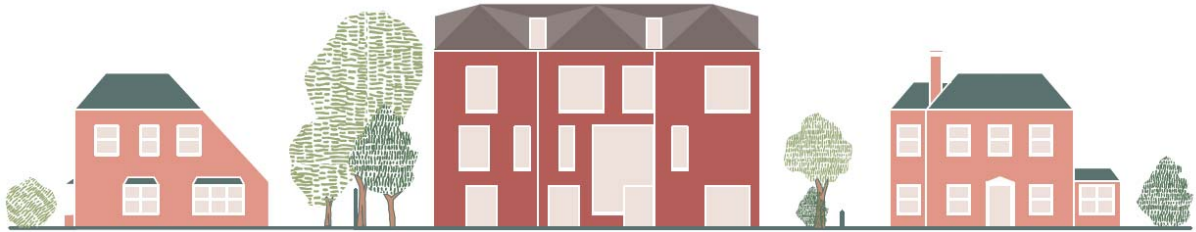


Figure 3: Extract from the SDG



Figure 4: Proposed street scene

- 8.10 In respect of the flatted building to the front of the site this would feature 4 storeys (through utilising land level changes across the site by way of a lower ground storey facing onto the rear garden), however would only feature 3 storeys on the street facing elevation. The ridge height of this building would sit at a slightly higher level to that of 24 Purley Knoll and around half a storey higher than that of 20 Purley Knoll, however such differentiations in height between neighbouring properties in the local context are not uncommon, and in this instance are not of an extent that would result in a negative impact upon the local character. Whilst the SDG allows proposals in locations such as this to have up to 3 storeys with an additional floor contained within the roof space or set back from the building envelope below, given the prominence of this site atop the brow of a hill and its relationship with 20 Purley Knoll (which is of a lower height than many of the surrounding properties), it is considered that the scale, height and massing of the flatted building to the front of the site strikes an appropriate balance between respecting the local character whilst intensifying and optimising the development potential of the site, and is thus acceptable.

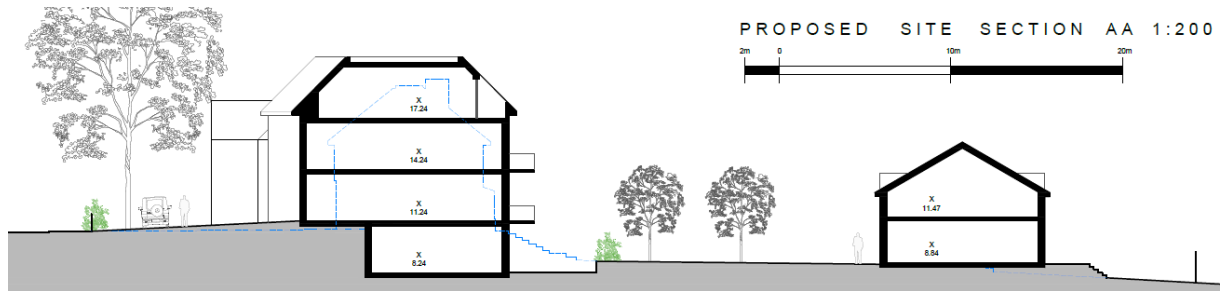


Figure 5: Proposed site section

8.11 To the rear of the site a pair of semi-detached houses of two storeys in height are proposed, being half the height of the flatted block to the front of the site and featuring a relatively modest footprint, meaning that they would read as being visually subordinate to the flatted block. Their subservience to both the flatted block and the existing surrounding properties fronting Purley Knoll would be further reinforced by the fact that this part of the site sits circa 2.5m lower than the front of the site (see Figure 5). Given the notable subservience of the semi-detached houses to the rear of the site, their scale, height and massing is acceptable and would accord with the relevant sections of the CLP and the SDG.

Appearance and Materials

8.12 The proposed architectural approach for the development incorporates a mixture of sympathetic and faithful and contemporary reinterpretation elements (as defined in section 2.8 of the SDG), through respecting and referencing the surrounding character of the area, in terms of the building's form, material palette and detailing, whilst at the same time introducing aspects of contemporary detailing to ensure that the proposal is not simply a pastiche of surrounding buildings. Such an approach to the appearance of the proposed development is considered appropriate in this instance and would complement and respect the character of the surrounding area.

8.13 The proposed roof forms of both the flatted block and the semi-detached properties would be pitched, featuring gables, hipped roofs and dormer windows (features found within the local area). The respective features of the roof forms are well proportioned against the rest of the proposal, are not overcomplicated and are well considered, and accord with the relevant guidance set out in the SDG.

8.14 The proposed material palette consists of brick, stone and tile hanging, and features detailing to the eaves (incorporating bargeboards and fascia's) and decorative roof brackets, taking reference from the surrounding built environment. The proposed material palette and detailing is well considered, would complement the surrounding character, and would accord with relevant guidance in the SDG. A condition requiring the submission of samples and the specification of the final materials, alongside detailed drawings of reveal depths and key junctions/features (such as the decorative roof brackets) has been recommended. A mixture of setts and slabs would form the proposed hardscaping alongside soft landscaping features, with a condition requiring the submission of samples of such alongside details of maintenance measures and any external lighting being recommended.

8.15 In order to respect the character of the street, balconies have not been proposed to the front elevation, with all of the proposed balconies proposed to the rear elevation,

featuring metal balustrading to their fronts, and 'hit and miss' brickwork walls of 1.8m in height to their sides. The proposed balcony design has been treated as an integral part of the design and follows the guidance set out in the SDG.

Housing Quality

8.16 As outlined by Figure 6 below all of the proposed units would meet or exceed the internal floor area and private amenity space standards set out by both the Nationally Described Space Standards (NDSS) and Table 6.2 in the CLP.

	Internal Floor Area		Private Amenity Space	
	Requirement	Proposed	Requirement	Proposed
FLAT 1 (3B4P)	<i>74sqm</i>	85sqm	<i>7sqm</i>	37sqm
FLAT 2 (2B4P)	<i>70sqm</i>	74sqm	<i>7sqm</i>	7sqm
FLAT 3 (2B3P)	<i>61sqm</i>	79sqm	<i>6sqm</i>	10sqm
FLAT 4 (2B4P)	<i>70sqm</i>	74sqm	<i>7sqm</i>	7sqm
FLAT 5 (2B4P)	<i>70sqm</i>	72sqm	<i>7sqm</i>	12sqm
FLAT 6 (1B2P)	<i>50sqm</i>	57sqm	<i>5sqm</i>	8sqm
FLAT 7 (1B2P)	<i>50sqm</i>	57sqm	<i>5sqm</i>	7sqm
HOUSE 1 (3B4P)	<i>84sqm</i>	87sqm	<i>7sqm</i>	75sqm
HOUSE 2 (3B4P)	<i>84sqm</i>	87sqm	<i>7sqm</i>	75sqm

Figure 6: Internal floor area and private amenity space

8.17 All of the proposed units would be either dual or triple aspect, incorporate dedicated storage and would provide a high standard of accommodation for their future occupiers. The distance between the rear elevation of the flatted block at the front of the site and the front elevation of the semi-detached houses at the rear of the site would be in excess of 18m and is acceptable in terms of any overlooking impacts between these two buildings.

8.18 Communal amenity space and child play space is located between the flatted block and the semi-detached houses and can be directly accessed from within the flatted block via a footpath on the north-west side of the building as well as the semi-detached houses in accordance with the guidance contained within the SDG. In line with Table 6.2 of the CLP the proposed development would be required to provide a minimum of 19.2sqm of child play space, and the proposed development would provide an area dedicated to child play space in excess of this. A condition requiring the submission of further information and details in relation to the proposed communal amenity space and child play space is recommended.

8.19 In terms of accessibility, level access would be provided from the front door to the ground floor units (which includes 1 x 2b3p and 1 x 2b4p apartments), with the internal layout of the 2b3p unit designed to be wheelchair adaptable/accessible (in accordance with Part M4(3) of the Building Regulations) and the internal layout of the 2b4p unit designed to be adaptable/accessible (in accordance with Part M4(2) of the Building Regulations). Such provision would be secured via condition. Given the limitations of the footprint and the overall scale of development proposed, it has not been possible to provide a lift for this development and as such the above and below ground floor units would not be fully accessible. The LP however states that accessible unit provisions should be applied flexibly to developments of four stories or less to ensure that such development is deliverable and the lack of a lift in this development is thus acceptable. It is recognised that the communal garden and child play area to the rear of the flatted block would not feature step free access, however given the notable land level changes on this site and its shape and size, an alternative arrangement to that proposed has not been possible. In respect of the semi-detached houses to the rear of the site, it is recognised that access to them is via a stepped pathway, however such an arrangement is necessary due to the notable change in land levels across the site. Said stepped access has however been designed in a manner to make this route as comfortable as possible for its users through the use of small flights of steps interspersed by generous flat landings, and is acceptable. A blue badge car parking space with the necessary clear zone around it has been proposed to the front of the site.

Impact on Surrounding Neighbours

8.20 There are a number of properties that surround the site which include 20 Purley Knoll to the south-east, 24 Purley Knoll to the north-west, 23 and 25 Purley Knoll to the south-west and 29 and 31 Banstead Road to the north-east as illustrated in Figure 7 below.



Figure 7: Surrounding neighbours



Figure 8: Rear elevation with 45 degree line

20 Purley Knoll

- 8.21 20 Purley Knoll sits on the south-east side of the application site, features a gated side pathway along its boundary with the application site and does not feature any side windows on its north-west elevation (i.e. the flank elevation adjacent to the application site). The front building line of the proposed flatted block would sit 4.5m forward of the front elevation of 20 Purley Knoll (which sits notably behind the predominant building line of the street), however due to the notable distance between the proposed flatted block and the closest habitable room window on the front elevation of 20 Purley Knoll, the proposed flatted block would not result in unacceptable amenity impacts for 20 Purley Knoll at its front. The rear building line of the proposed flatted block would sit 7m beyond the rear elevation of 20 Purley Knoll, however a 45 degree line (taken from the centre point of the closest habitable room window on the rear elevation of 20 Purley Knoll outlined in Figure 8) demonstrates that the proposed flatted block would not breach this line in either plan nor elevation (in accordance with the SDG) and as such the proposed flatted block would not result in unacceptable amenity impacts for 20 Purley Knoll at its rear.
- 8.22 The side elevation of the proposed flatted block facing 20 Purley Knoll features three windows at ground floor level, and two windows at first floor level. A condition is recommended requiring that both of the first floor windows would need to be obscured glazed and non-openable below 1.7m (above internal floor level) in order to protect the amenity of 20 Purley Knoll. Whilst the three windows at ground floor level would not need to be included within the aforementioned condition in order to protect the amenity of 20 Purley Knoll, it should be noted that in this instance they would too also be required to be obscured glazed and non-openable below 1.7m (above internal floor level) in order to protect the amenity of future occupiers of this apartment due to the presence of the adjacent pathway. With respect to the balconies/terraces that sit to the rear of the flatted block adjacent to 20 Purley Knoll, the terrace at lower ground level would sit well below the existing fence line, whilst the outboard balconies at ground and first floor level would feature 1.8m high 'hit and miss' brickwork walls to their sides to prevent direct overlooking. The balcony at second floor level would be inset within the building's roof form thus preventing direct overlooking.
- 8.23 In respect of the proposed semi-detached houses at the rear of the site, these would be sited 26m behind the rear building line of 20 Purley Knoll (exceeding the 18m distance requirement in the SDG) and feature no side windows. Whilst it is recognised that first floor windows on their front elevations (facing back towards 20 Purley Knoll) are proposed, the closest window to the boundary, which serves a bathroom, would be obscured glazed and non-openable below 1.7m (which would be secured via condition), with the next closest window, serving a bedroom, sitting 5.75m in from the boundary with 20 Purley Knoll. The above considerations would ensure that there would be no direct overlooking of 20 Purley Knoll from the semi-detached properties with any indirect overlooking limited to an acceptable level not uncommon in a suburban environment such as this.

24 Purley Knoll

- 8.24 24 Purley Knoll sits on the north-west side of the application site, features a side pathway and single storey side extension (attached to the existing property on the application site) along its boundary with the application site, with the only side window on its south-east elevation (i.e. the flank elevation adjacent to the application site) being

a small obscured glazed window at ground floor level (which would not be afforded the same level of protection as a primary habitable room window). The front building line of the proposed flatted block would sit slightly behind the front elevation of 24 Purley Knoll and would therefore not result in unacceptable amenity impacts for 24 Purley Knoll at its front. The rear building line of the proposed flatted block would sit 3.5m beyond the rear elevation of 24 Purley Knoll, and whilst the 45 degree line would not be breached in plan, nor in elevation (as illustrated on the rear elevation), officers have since adjusted the location of the 45 degree line in elevation (such that it is taken from the centre point of the closest habitable room window) which now shows that the proposal would marginally breach this line. Given however that this window would only marginally fail in elevation, the impact on daylight and sunlight levels for 24 Purley Knoll would not be significant due to the orientation of the two properties in relation to one another and as such the proposed flatted block would not result in unacceptable amenity impacts for 24 Purley Knoll at its rear.

- 8.25 The side elevation of the proposed flatted block facing 24 Purley Knoll features two windows at ground floor level, and two windows at first floor level. A condition is recommended requiring that both of the first floor windows would need to be obscured glazed and non-openable below 1.7m (above internal floor level) in order to protect the amenity of 24 Purley Knoll. With respect to the balconies/terraces that sit to the rear of the flatted block adjacent to 24 Purley Knoll, the terrace at lower ground level would sit well below the existing fence line, whilst the outboard balconies at ground and first floor level would feature 1.8m high 'hit and miss' brickwork walls to their sides to prevent direct overlooking. The balcony at second floor level would inset within the building's roof form thus preventing direct overlooking.
- 8.26 In respect of the proposed semi-detached houses at the rear of the site, these would be sited 26m behind the rear building line of 24 Purley Knoll and feature no side windows. Whilst it is recognised that first floor windows on their front elevation (facing back towards 24 Purley Knoll) are proposed, the closest window to the boundary, which services a bathroom, would be obscured glazed and non-openable below 1.7m (which would be secured via condition), with the next closest window, serving a bedroom, sitting 5.5m in from the boundary with 24 Purley Knoll. The above considerations would ensure that there would be no direct overlooking of 24 Purley Knoll from the semi-detached properties with any indirect overlooking limited to an acceptable level not uncommon in a suburban environment such as this.

23 and 25 Purley Knoll

- 8.27 These properties are located opposite the site with the separation distance between these properties and the proposal being around 30m and including the presence of a road and landscaping. Given the degree of separation between the proposed development and these properties, the proposals would not result in unacceptable amenity impacts for these properties.

29 and 31 Banstead Road

- 8.28 These properties are located to the rear of the site with the separation distance between these properties and the semi-detached properties at the rear of the site being in excess of 45m. Furthermore there is significant established vegetation located between the application site and these properties which is to be retained. As such the proposals would not result in unacceptable amenity impacts for these properties.

Highways, Parking and Refuse

- 8.29 The site has a PTAL of 5 representing a very good level of public transport accessibility, sits approximately 600m west of Purley rail station, and sits approximately 250m from the closest bus stop on Brighton Road which is served by a number of high frequency bus routes. Purley Knoll is a non-classified residential two-way side street terminated by Woodcote Valley Road in the west and Brighton Road (A23) in the east. The site falls within the Purley Controlled Parking Zone (CPZ) which operates on Monday to Saturday between 9am and 5pm.
- 8.30 Vehicular access to the site is currently via a driveway and dropped kerb on the east side of the site (adjacent to 20 Purley Knoll). The proposed development seeks to retain vehicular access to the site in the same location (albeit through a slightly widened access point) with further details of the proposed widened access point, including visibility splays being required by condition. Works to widen the existing vehicle crossover to take into account the widened access point will be carried out under a S.278 agreement between the applicant and the Council. Vehicular tracking diagrams have been provided with the application and demonstrate that sufficient space to accommodate the necessary vehicular movements within the forecourt at the front of the site has been provided.
- 8.31 Table 6.2 of the LP sets out maximum car parking standards for residential developments. This states that 1-2 bedroom properties should provide less than 1 space per unit, with up to 1.5 spaces per unit being provided for 3 bedroom properties. In line with the LP the proposed development could therefore provide up to a maximum of 11 spaces (rounded up from 10.5). It is important to note however that it is not necessarily desirable to provide car parking up to the maximum standards given the ambitions of both the LP and CLP to reduce reliance on car usage and promote/prioritise sustainable modes of transport. This position is especially relevant in this case given the site's very good level of public transport accessibility and its location very close to a District Centre and within a CPZ which allows the Council to restrict future occupiers of the proposed development from applying for parking permits. As such a significantly lower level of on-site car parking can be supported in this instance in the interests of reducing the development's reliance on car usage.
- 8.32 The proposed development provides a total of 4 off-street parking spaces (including 1 blue badge space and electric vehicle charging facilities), and given that census data projects the proposed development as giving rise to 7-8 cars, the proposal could potentially lead to a parking 'overspill' of 3-4 vehicles which would usually need to be accommodated on-street in the locality. The applicant has undertaken a parking stress survey in accordance with the Lambeth Methodology, which demonstrates that existing parking stress levels on Purley Knoll are relatively low (with generally 30+ spaces available overnight) and thus could accommodate the anticipated overspill. Given however the site's location within a CPZ and its very good level of public transport accessibility, it is recommended in this instance that a permit free agreement (imposed via a S.106 agreement) is proposed to prevent future occupiers of the development from applying for parking permits thus negating any potential parking 'overspill' for Purley Knoll. In light of the above the proposed car parking provision and the proposal's impact upon the local highway network would be acceptable.
- 8.33 In respect of cycle parking, the proposed development seeks to provide a total of 16 cycle parking spaces, in the form of 12 secure cycle boxes (for residents of the

proposed flatted block) located to the rear of the flatted block within the communal garden, and 2 secure cycle boxes for each of the semi-detached houses at the rear of the site, located within their respective private gardens. The overall quantum of cycle parking proposed would accord with LP and CLP standards, and a cycle ramp would be attached to the external stairs within the side pathway to ensure ease of access. A condition is recommended requiring further details of the proposed cycle storage and cycle ramp.

8.34 With respect to refuse storage, a dedicated refuse store located within the building envelope has been provided on the east side of the flatted block, accessed via the side pathway. The proposed refuse store would provide the requisite level of refuse storage for the proposed development and would accord with policy DM13.1 of the CLP.

Trees, Landscaping, Biodiversity and Sustainability

8.35 The existing site currently accommodates a total of 11 trees and hedges as identified by the tree survey schedule contained within the submitted Arboricultural Report. These include 1 Category B tree; 8 Category C trees and 2 Category U trees, and are highlighted in Figure 8 below. None of these trees are the subject of a Tree Preservation Order (TPO).

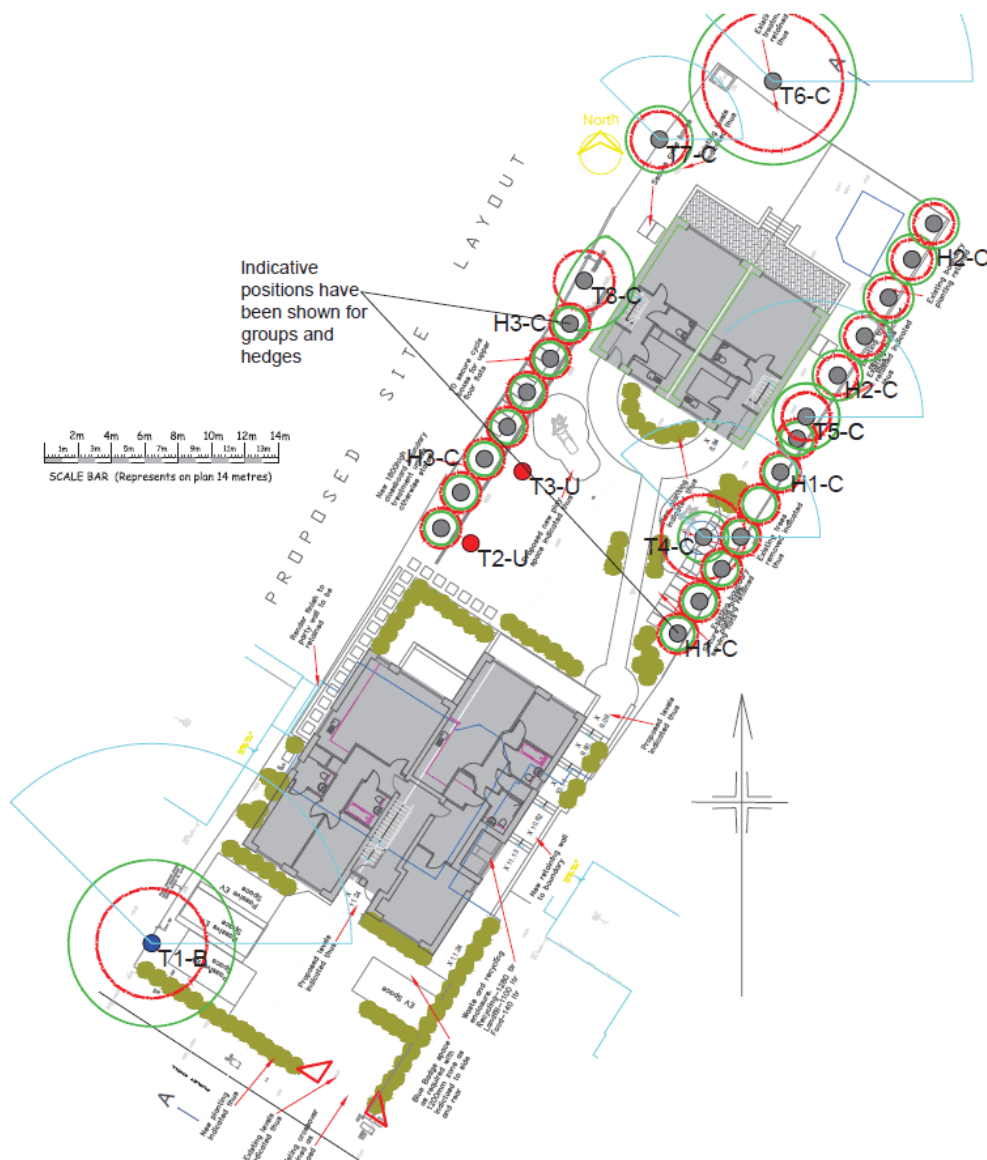


Figure 9: Existing tree survey

- 8.36 The proposed development seeks to remove 5 of the existing trees and hedges, 3 of which are classed as Category C trees, with the others being classed as Category U trees. Given the low quality of the trees that are proposed to be removed, their removal is acceptable. In order to mitigate for the loss of the aforementioned trees and improve the landscaping quality of the site, the proposal seeks to plant an additional 13 trees (resulting in a net uplift of 8 trees on site) which is welcomed. Conditions are recommended to secure the proposed additional tree planting and ensure that the proposed development accords with the tree protection measures for the retained trees as recommended in the submitted Arboricultural Report.
- 8.37 In respect of landscaping, whilst large portions of the site will feature soft landscaping in the form of lawns and planting, with details of the proposed planting having been submitted, hard landscaping in the form of setts and slabs is proposed to the front of the site, as well as along the side and to the rear of the site. Further details of the hardscaping (including samples) alongside details of maintenance measures and external lighting will be secured via condition.
- 8.38 A preliminary ecology report has been submitted in support of the application to identify what habitats are present on site and look for any evidence of, or potential for, protected/notable species. This report concluded that whilst the site contains common/widespread habitat types, none of these are of a conservation concern. In order to ensure that the proposed development does not have an adverse impact upon the biodiversity value of the site a series of mitigation measures in respect of the boundary hedgerows, breeding birds and reptiles have been recommended alongside a series of measures to enhance the biodiversity value of the site in accordance with policy DM27 of the CLP. A condition requiring that the development is carried out in strict accordance with the recommendations of this report is recommended.
- 8.39 Whilst no evidence of bats were recorded during the preliminary ecology survey, given the existing property's potential for bats a further bat survey was carried out. This subsequent bat survey identified the presence of a bat roost of low conservation significance. In order to avoid direct impacts to individual bats as a result of the proposed development a series of mitigation measures have been proposed, and a condition requiring that the development is carried out in strict accordance with the recommendations of this report is recommended.
- 8.40 In respect of sustainability conditions are recommended to ensure that the proposed development achieves a 19% reduction in CO2 emissions over the 2013 Building Regulations requirements and meets a minimum water efficiency standard of 110 litres/person/day in accordance with policy SP6.3 of the CLP.

Flood Risk and Sustainable Drainage

- 8.41 The applicant has submitted a Flood Risk and Surface Water Management Statement in support of the application. The site is located in Flood Zone 1 and according to the Environment Agency has a very low probability of fluvial or tidal flooding. Whilst the site does lie within a Critical Drainage Area as well as a location which has the potential for groundwater flooding to occur at surface, it should be noted that due to the site's elevation at the top of Purley Knoll, the site itself is at a low risk from surface water flooding and groundwater flooding.

8.42 Sustainable drainage measures are proposed on the site in accordance with policy DM25 of the CLP in the form of a crate soakaway to the rear of the proposed flattened block along with the use of permeable paving. A condition has been recommended requiring further details of the proposed sustainable drainage measures.

Conclusion

8.43 All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted subject to conditions and a legal agreement for the reasons set out above. The details of the decision are set in the RECOMMENDATION section of this report.

Appendix 1: Planning Policies and Guidance

The following lists set out the most relevant policies and guidance, although they are not exhaustive and the provisions of the whole Development Plan apply (in addition to further material considerations).

CLP

The Croydon Local Plan was adopted in February 2018 and the most relevant policies to this application are as follows:

- SP2 Homes
- DM1 Housing Choice for Sustainable Communities
- SP4 Urban Design and Local Character
- DM10 Design and Character
- DM13 Refuse and Recycling
- SP6 Environment and Climate Change
- DM23 Development and Construction
- DM25 Sustainable Drainage Systems and Reducing Flood Risk
- SP7 Green Grid
- DM27 Protecting and Enhancing our Biodiversity
- DM28 Trees
- SP8 Transport and Communication
- DM29 Promoting Sustainable Travel and Reducing Congestion
- DM30 Car and Cycle Parking in New Development
- DM42 Purley

The Suburban Design Guide was adopted in April 2019 as a supplementary planning document to the CLP and is of relevance.

LP

The London Plan was adopted in March 2016 and the most relevant policies to this application are as follows:

- Policy 3.3 Increasing Housing Supply
- Policy 3.4 Optimising Housing Potential
- Policy 3.5 Quality and Design of Housing Developments
- Policy 3.6 Children and Young People's Play and Informal Recreation Facilities
- Policy 3.8 Housing Choice
- Policy 5.2 Minimising Carbon Dioxide Emissions
- Policy 5.3 Sustainable Design and Construction
- Policy 5.12 Flood Risk Management
- Policy 5.13 Sustainable Drainage
- Policy 5.15 Water Use and Supplies
- Policy 5.17 Waste Capacity
- Policy 6.1 Strategic Approach
- Policy 6.3 Assessing Effects of Development on Transport Capacity
- Policy 6.9 Cycling
- Policy 6.13 Parking
- Policy 7.1 Lifetime Neighbourhoods

- Policy 7.2 An Inclusive Environment
- Policy 7.3 Designing Out Crime
- Policy 7.4 Local Character
- Policy 7.6 Architecture
- Policy 7.19 Biodiversity and Access to Nature
- Policy 7.21 Trees and Woodlands

The Housing SPG was adopted in March 2016 and the Play and Informal Recreation SPG was adopted in September 2012, both as supplementary planning guidance to the LP and are of relevance.

PLANNING COMMITTEE AGENDA

PART 8: Other Planning Matters

1 INTRODUCTION

- 1.1 In this part of the agenda are reports on planning matters, other than planning applications for determination by the Committee and development presentations.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 The following information and advice applies to all those reports.

2 FURTHER INFORMATION

- 2.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

3 PUBLIC SPEAKING

- 3.1 The Council's constitution only provides for public speaking rights for those applications being reported to Committee in the "Planning Applications for Decision" part of the agenda. Therefore reports on this part of the agenda do not attract public speaking rights.

4 BACKGROUND DOCUMENTS

- 4.1 For further information about the background papers used in the drafting of the reports in part 7 contact Mr P Mills (020 8760 5419).

5 RECOMMENDATION

- 5.1 The Committee to take any decisions recommended in the attached reports.

This page is intentionally left blank

PLANNING COMMITTEE AGENDA

21st November 2019

Part 8 Other Planning Matters

Item 8.1

Report of: Director of Planning and Strategic Transport Author: Pete Smith	Title: Weekly Planning Decisions
---	--

1. Purpose

- 1.1 This report provides a list of cases determined (since the last Planning Committee) providing details of the site and description of development (by Ward), whether the case was determined by officers under delegated powers or by Planning Committee/Sub Committee and the outcome (refusal/approval).

Planning Decisions

- 1.2 Attached as Appendix 1 is the list of delegated and Planning Committee/Sub Committee decisions taken between 28th October 2019 and 8th November 2019.
- 1.4 During this period the service issued 169 decisions (ranging from applications for full planning permission, applications to discharge or vary planning conditions, applications for tree works, applications for prior approval, applications for non-material amendments and applications for Certificates of Lawful Development). 9 applications were withdrawn by the applicants (which also appear on the list). Incidentally,
- 1.5 One of these withdrawn applications involved the proposed redevelopment of 34A and 34B Arkwright Road – involving the erection of a 3/4 storey building comprising 23 flats with alterations to the existing vehicular access road and the formation of a parking areas, amenity space, cycle and refuse storage (LBC Ref 19/03643/OUT) which had generated significant local comment .
- 1.6 Out of the 169 decisions issued, 23 were refused (13.6%). Therefore the approval rate for last week was 86.4%.
- 1.7 Notable decisions are listed below
- Following the completion of the associated S.106 Agreement, planning permission was issued in respect of the redevelopment of 40-60, 42 and 42A Cherry Orchard Road (Meatpackers) comprising 120 residential units, contained within a 7 to 9 storey building (LBC Erf 18/03320/FUL). This followed on from the resolution to grant planning permission by the Planning Committee at its meeting of the 28th February 2019.

- Following the completion of the associated S.106 Agreement, planning permission was issued in respect of the mixed use redevelopment of 5-9 Surrey Street comprising flexible commercial space at lower ground and ground floor level, with 63 residential units above, contained within a part 6, part 7, part 8 storey building (LBC Ref 18/01211/FUL). This followed on from the resolution to grant planning permission by the Planning Committee at its meeting of the 14th February 2019.
- Planning permission has been refused for the redevelopment of 48 Grasmere Road comprising 8 residential units contained within a 3 storey building (LBC Ref 19/04376/FUL). The reasons for refusal focussed on the scheme's failure to respect the character and appearance of the area and the increased pressure that would be placed on existing on street car parking conditions.

DEVELOPMENT MANAGEMENT - PLACE DEPARTMENT

DELEGATED PLANNING DECISIONS (Ward Order)

The following is a list of planning applications determined by the Head of Development Management under delegated powers since the last meeting of the Planning Committee.

Note: This list also includes those decisions made by Planning Committee and released in this time frame as shown within the level part of each case.

NOTE: The cases listed in this report can be viewed on the Council's Website.

Please note that you can also view the information supplied within this list and see more details relating to each application (including the ability to view the drawings submitted and the decision notice) by visiting our Online Planning Service at the Croydon Council web site (www.croydon.gov.uk/onlineplans).

Once on the Council web page please note the further information provided before selecting the Public Access Planning Register link. Once selected there will be various options to select the Registers of recently received or decided applications. Also; by entering a reference number if known you are able to ascertain details relating to a particular application. (Please remember to input the reference number in full by inserting any necessary /'s or 0's)

Ref. No. :	18/03320/FUL	Ward :	Addiscombe West
Location :	40-60, 42 & 42A Cherry Orchard Road Croydon CR0 6BA	Type:	Full planning permission

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Ref. No. : 19/04309/DISC **Ward : Addiscombe West**
Location : Land Adjacent To East Croydon Station And Type: Discharge of Conditions
Land At Cherry Orchard Road, Cherry
Orchard Gardens, Billington Hill, Croydon.
Proposal : Discharge of condition 27 attached to planning permission 17/05046/FUL for the Erection of two 25 storey towers (plus plant) and a single building ranging from 5 to 9 storeys (plus plant) to provide a total of 445 residential units, with flexible commercial, retail and community floorspace (A1/A2/A3/A4/B1a/D1/D2) at ground and first floor level of the two towers and associated amenity, play space, hard and soft landscaping, public realm, cycle parking and car parking with associated vehicle accesses

Date Decision: 30.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04311/DISC **Ward : Addiscombe West**
Location : Land Adjacent To East Croydon Station And Type: Discharge of Conditions
Land At Cherry Orchard Road, Cherry
Orchard Gardens, Billington Hill, Croydon.
Proposal : Discharge of conditions 35 and 36 attached to planning permission 17/05046/FUL for the Erection of two 25 storey towers (plus plant) and a single building ranging from 5 to 9 storeys (plus plant) to provide a total of 445 residential units, with flexible commercial, retail and community floorspace (A1/A2/A3/A4/B1a/D1/D2) at ground and first floor level of the two towers and associated amenity, play space, hard and soft landscaping, public realm, cycle parking and car parking with associated vehicle accesses

Date Decision: 30.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04602/FUL **Ward : Addiscombe West**
Location : 42A Morland Avenue Type: Full planning permission
Croydon
CR0 6EA
Proposal : Erection of a dormer extension in the rear roof slope, roof lights in the front roof slope and internal reconfiguration of the first floor flat to facilitate the creation of 1 x 2 bedroom flat in the roof space

Date Decision: 31.10.19

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/04825/CAT **Ward : Addiscombe West**
Location : 79 Clyde Road Type: Works to Trees in a
Croydon Conservation Area
CR0 6SZ

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Proposal : Discharge of Condition 5a (Confirmation of principle facing brick) attached to planning permission 18/06102/FUL for the redevelopment of the site to provide 137 residential units across an 8 and 18 storey building with associated landscaping and access arrangements.

Date Decision: 06.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04300/HSE
Location : 51 Peshurst Road
Thornton Heath
CR7 7EE
Proposal : Erection of single storey rear extension
Ward : **Bensham Manor**
Type: Householder Application

Date Decision: 31.10.19

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/04456/LP
Location : 95 Peshurst Road
Thornton Heath
CR7 7EB
Proposal : Erection of loft conversion with dormer in the rear roof slope and roof lights in the front roof slope.
Ward : **Bensham Manor**
Type: LDC (Proposed) Operations edged

Date Decision: 08.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/03110/FUL
Location : 1 Thomson Crescent
Croydon
CR0 3JT
Proposal : Erection of three bedroom detached house at side: formation of vehicular access and provision of associated parking on land adjoining 1 Thomson Crescent.
Ward : **Broad Green**
Type: Full planning permission

Date Decision: 29.10.19

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Proposal : Discharge of Conditions for the Legacy Buildings Only: Condition 1 (boilers), 60 (contamination), 61 (surface water), 65 (ecology), 66 (tree protection), 68 (temp buildings for construction) 73 (CLP), 78 (ECMP), 79 (low emissions) and 81 (scheme for chapel, water tower and admin buildings) and Part Discharge of Condition 72 (Part A only) (archaeology) and Part Discharge of Condition 77 (carbon) attached to planning application 13/02527/P for the Redevelopment of the former Cane Hill Hospital Site to accommodate up to 677 residential units (net increase of 675 units); Class A1-A5; B1; C1; D1-D2 Uses; car and cycle parking provision, landscaping and public realm works, interim works, and highway works including a new access onto Marlpit Lane/ Brighton Road Roundabout and Portnalls Road comprising: Outline planning application for the retention and re-use of the Water Tower and Chapel and Refurbishment and Re-use of Administration Building for Class A1-A5; B1; C3; D1-D2 purposes; Re-Use/Rebuild of North Lodge as Use Class C3 single dwelling house; Relocation of Farm and Change of use of Glencairn from Use Class C2 to a Use Class C3 dwelling house, refurbishment and change of use of MSU building for farming purposes, and erection of three barns on tennis court site; a single building of 3,000m² GEA for Office (B1) or Hotel (C1) uses; up to 473 new residential units (Class C3); and new access onto Portnalls Road and re-use of existing access onto Portnalls Road. Full planning application for 187 residential units (Class C3) and engineering operations comprising a new road and access from the Marlpit Lane / Brighton Road (A237) Roundabout and associated infrastructure including drainage.

Date Decision: 05.11.19

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 19/03630/HSE
Location : 2 Nutfield Road
Coulsdon
CR5 3JN
Ward : Coulsdon Town
Type: Householder Application
Proposal : Erection of a single storey side and rear extension.

Date Decision: 29.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03976/HSE
Location : 55 South Drive
Coulsdon
CR5 2BJ
Ward : Coulsdon Town
Type: Householder Application
Proposal : Erection of a single storey rear extension and a rear dormer

Date Decision: 01.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04553/CONR **Ward : Coulsdon Town**
Location : Aldi Stores Ltd, Type: Removal of Condition
159 Brighton Road
Coulsdon
CR5 2NH
Proposal : Erection of two storey building comprising retail foodstore (A1) at ground floor with D1 (training) and B1 (office) at first floor level; provision of associated highway and parking facilities (without compliance with condition 9- hours of use - attached to planning permission 16/00450/P)

Date Decision: 01.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04771/DISC **Ward : Coulsdon Town**
Location : 5-7 Leaden Hill And Leaden Hill Industrial Type: Discharge of Conditions
Estate, 9 Leaden Hill, Coulsdon, CR5 2BQ
Proposal : Discharge of condition 14 (contamination) and 16 (verification) attached to planning permission 15/05723/P for Erection of a mixed use development of between 65 and 75 residential units and 450 and 499m2 office use (B1a)

Date Decision: 31.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05075/DISC **Ward : Coulsdon Town**
Location : 41 Chipstead Valley Road Type: Discharge of Conditions
Coulsdon
Proposal : Discharge of condition 4 (details of bin and bike store) and 5 (refuse collection arrangements) attached to application 19/03467/GPDO for the Change of use from Office (Class B1(a)) to residential (C3) to provide 10 studio flats

Date Decision: 08.11.19

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 18/01211/FUL **Ward : Fairfield**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 5-9 Surrey Street
Croydon
CR0 1RG
Type: Full planning permission

Proposal : Demolition of the existing building and replacement with a six/seven/eight storey development. Commercial units would be provided on the lower ground (sui generis, A3 and D1/D2) and ground floor (flexible use A1, A2, A3, D1, D2, B1(a)), with 60 flats above with associated public realm improvements and landscaping including courtyard area with, disabled car parking and cycle parking.

Date Decision: 08.11.19

P. Granted with 106 legal Ag. (3 months)

Level: Planning Committee

Ref. No. : 18/06169/DISC
Location : Segas House
Katharine Street
Croydon
CR0 1NX
Ward : **Fairfield**
Type: Discharge of Conditions

Proposal : Application to discharge Condition 7d (ground floor room proposed finishes) of 18/01861/LBC Proposed works including internal alterations, replacement of shopfront, and other associated works to facilitate the change of use of the ground floor to a marketing suite for a temporary period of five years

Date Decision: 30.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/02763/FUL
Location : 66 Church Street
Croydon
CR0 1RB
Ward : **Fairfield**
Type: Full planning permission

Proposal : Replacement of existing pharmacy shopfront. Demolition of rear linked outbuilding storage facility. Conversion of existing upper floors to provide two self-contained flats each with rear extension addition. Erection of separate three storey building to provide 3 x 1-bed self-contained flats. Provision of cloistered rear landscaped courtyard.

Date Decision: 01.11.19

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/03450/DISC
Ward : **Fairfield**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : Crown And Pepper
242 High Street
Croydon
CR0 1NF

Type: Discharge of Conditions

Proposal : Discharge of Conditions 3 and 5, and Part-Discharge of Condition 9 attached to Planning Permission 18/05097/FUL for Erection of a three storey side extension (with ground floor undercroft), one and two storey rear extensions, alterations and extensions to the front and rear roofslopes and the erection of a third floor above, to provide 1 no. three bed flat, 6 no. one bed flats and 2 no. studio flats across the upper floors, and an enlarged public house on the ground floor and to the rear, together with associated beer garden, landscaping, bin and cycle stores.

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04178/DISC
Location : 5 Cairo New Road
Croydon
CR0 1XP

Ward : **Fairfield**
Type: Discharge of Conditions

Proposal : Discharge of conditions 10 and 11 attached to permission 15/04748/P for the Demolition of existing buildings; Erection of a 4/14 (with plant above) storey building comprising 43 two bedroom, 32 studio, 30 one bedroom and 8 three bedroom flats, a use within A3 (restaurant/cafe) and D1 (non residential institution) on the ground floor and new public square including area for commercial use

Date Decision: 30.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04247/DISC
Location : Segas House
Katharine Street
Croydon
CR0 1NX

Ward : **Fairfield**
Type: Discharge of Conditions

Proposal : Application to re-discharge Condition 7b and c (in relation to parquet flooring) of 18/01861/LBC Proposed works including internal alterations, replacement of shopfront, and other associated works to facilitate the change of use of the ground floor to a marketing suite for a temporary period of five years

Date Decision: 30.10.19

Approved

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Date Decision: 29.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04248/DISC **Ward : Kenley**
Location : 32 Welcomes Road **Type: Discharge of Conditions**
Kenley
CR8 5HD
Proposal : Discharge of condition 11 (Construction Logistics Plan) attached to planning permission 18/05006/FUL for, Demolition of existing building: erection of a three storey building comprising 9 units and formation of associated vehicular access and provision of 9 off-street parking spaces, cycle storage and refuse store.

Date Decision: 30.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04359/HSE **Ward : Kenley**
Location : 25 Highland Road **Type: Householder Application**
Purley
CR8 2HS
Proposal : Conversion of the existing garage to a habitable room and associated alterations including alteration to the roof line.

Date Decision: 30.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04651/NMA **Ward : Kenley**
Location : 45 Old Lodge Lane **Type: Non-material amendment**
Purley
CR8 4DL
Proposal : Non-material amendment (alterations to the external elevations/roof form and internal accommodation) linked to Planning Application 17/05104/FUL for the alterations. Erection of a single/two storey side/rear and roof extensions including accommodation with roof space. Conversion to form 5 units including associated soft and hard landscaping, bicycle, refuse/recycling storage and off-street parking.

Date Decision: 29.10.19

Approved

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Level: Delegated Business Meeting

Ref. No. : 19/04239/CONR **Ward : New Addington South**
Location : Garages To The Rear Of 122 - 124 Chertsey Type: Removal of Condition
Crescent
Croydon
CR0 0DB
Proposal : Variation of conditions 1 (s106 amendments - habitable rooms alterations) and 2 (plan numbers - alterations to house 7) linked to planning application 16/05998/FUL for the demolition of garages and erection of 4 two bedroom and 3 three bedroom houses with provision of landscaping and associated works.

Date Decision: 31.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04260/HSE **Ward : New Addington South**
Location : 30 Gascoigne Road Type: Householder Application
Croydon
CR0 0NA

Proposal : Alterations, erection of two storey rear extension

Date Decision: 05.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04315/HSE **Ward : New Addington South**
Location : 37 North Downs Road Type: Householder Application
Croydon
CR0 0LE

Proposal : Erection of a two storey side extension with front porch extension, installation of a new rear first floor window opening, erection of a rear timber pergola and associated works.

Date Decision: 08.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03538/FUL **Ward : Norbury Park**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : Land R/O 2-4 Green Lane
Thornton Heath
CR7 8BA
Type: Full planning permission

Proposal : Erection of a new two storey terrace comprising 3no two bedroom homes and a single storey rear extension to the existing commercial premises

Date Decision: 01.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03884/FUL
Location : 203 Norbury Avenue
Thornton Heath
CR7 8AB
Type: Full planning permission
Ward : **Norbury Park**

Proposal : Demolition and erection of single storey front extension/porch

Date Decision: 31.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04230/HSE
Location : 37 Highbury Avenue
Thornton Heath
CR7 8BP
Type: Householder Application
Ward : **Norbury Park**

Proposal : Erection of first floor side extension; alterations to include new windows to front and rear elevations and installation of rooflights in front roofslope

Date Decision: 29.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04417/GPDO
Location : 219 Green Lane
Norbury
London
SW16 3LY
Type: Prior Appvl - Class A Larger House Extns
Ward : **Norbury Park**

Proposal : Erection of a single storey rear extension which projects out 4.5 metres from the rear wall, with an eaves height of 3 metres and a maximum height of 3.5 metres

Date Decision: 29.10.19

Ref. No. : 19/03840/GPDO
Location : 13 Kilmartin Avenue
Norbury
London
SW16 4RE

Ward : Norbury And Pollards Hill
Type: Prior Appvl - Class A Larger House Extns

Proposal : Erection of a single storey rear extensions projecting out 4 metres with a maximum height of 3.197 metres

Date Decision: 05.11.19

(Approval) refused

Level: Delegated Business Meeting

Ref. No. : 19/04253/LP
Location : 13 Ederline Avenue
Norbury
London
SW16 4RZ

Ward : Norbury And Pollards Hill
Type: LDC (Proposed) Operations edged

Proposal : Alterations to the roof including the formation of dormer in the rear roof slope and rooflights in the front roof slope.

Date Decision: 31.10.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/04277/FUL
Location : 4 Beatrice Avenue
Norbury
London
SW16 4UN

Ward : Norbury And Pollards Hill
Type: Full planning permission

Proposal : Construction of single storey rear extension, alterations to roof including hip to gable loft conversion with dormers in the rear roof slope and roof lights in the front roof slope, and conversion of dwelling into five apartments 1 x 3 bedroom and 4 x 1 bedroom, with associated refuse and cycling storage and landscaping.

Date Decision: 08.11.19

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/04287/DISC
Ward : Norbury And Pollards Hill

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 222 Tylecroft Road
Norbury
London
SW16 4TQ

Type: Discharge of Conditions

Proposal : Discharge of condition 02 attached to planning permission 19/02158/HSE (alterations to include replacement windows to front and rear elevations, erection of single storey rear extension and installation of three rooflights in rear roofslope).

Date Decision: 04.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04347/GPDO
Location : 239 Norbury Crescent
Norbury
London
SW16 4LF

Ward : **Norbury And Pollards Hill**
Type: Prior Appvl - Class A Larger House Extns

Proposal : Erection of single storey rear extension projecting out a maximum of 6 metres from the original rear wall with a maximum height of 2.65 metres

Date Decision: 05.11.19

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 19/04394/FUL
Location : 132 Norbury Court Road
Norbury
London
SW16 4HY

Ward : **Norbury And Pollards Hill**
Type: Full planning permission

Proposal : Alterations and conversion of the dwelling into 1 three bedroom and 4 one bedroom flats.

Date Decision: 08.11.19

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/04471/FUL
Location : 64 Darcy Road
Norbury
London
SW16 4UA

Ward : **Norbury And Pollards Hill**
Type: Full planning permission

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Proposal : Discharge of Condition 3 (Written Scheme of Investigation) attached to permission 18/05894/HSE dated 01/03/2019 (Demolition of an air raid shelter, erection of a double storey garage)

Date Decision: 05.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/03975/TRE
Location : 17 Runnymede Court
17 Park Hill Rise
Croydon
CR0 5JF
Proposal : T2: Oak - Prune back lateral branches overhanging 'No 4C Paul Gardens' by up to 3m. (TPO no. 6, 2011)

Ward : Park Hill And Whitgift
Type: Consent for works to protected trees

Date Decision: 31.10.19

Consent Refused (Tree application)

Level: Delegated Business Meeting

Ref. No. : 19/04965/LP
Location : 19 Radcliffe Road
Croydon
CR0 5QG
Proposal : Erection of rear single storey extension

Ward : Park Hill And Whitgift
Type: LDC (Proposed) Operations edged

Date Decision: 01.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/03592/HSE
Location : 3 Yew Tree Walk
Purley
CR8 1HD
Proposal : Construction of a two storey side extension and single storey rear conservatory extension.

Ward : Purley Oaks And Riddlesdown
Type: Householder Application

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 123 Grange Road
South Croydon
CR2 0NF
Type: LDC (Proposed) Operations
edged
Proposal : Construction of hip to gable roof extension and rear dormer. Installation of 2 x front
rooflights.

Date Decision: 30.10.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/04251/HSE
Ward : **Purley Oaks And
Riddlesdown**
Location : 112 Whytecliffe Road North
Purley
CR8 2AS
Type: Householder Application
Proposal : Alterations, erection of a single storey side extension and raising of existing cill to the
ground floor rear bay window

Date Decision: 04.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04262/DISC
Ward : **Purley Oaks And
Riddlesdown**
Location : 7A Warren Road
Purley
CR8 1AF
Type: Discharge of Conditions
Proposal : Discharge of Condition 12 (Contaminated Land) attached to planning permission
17/03651/FUL for the demolition of the existing building, erection of a terrace comprising
of 6 x 4 bedroom two storey houses with accommodation in roofspace, erection of car
port and provision of associated car parking

Date Decision: 05.11.19

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/04376/FUL
Ward : **Purley Oaks And
Riddlesdown**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 46 Grasmere Road
Purley
CR8 1DU

Type: Full planning permission

Proposal : Demolition of existing dwellinghouse. Erection of three storey (replacement) building to provide 8 units (2 x 3 bed, 3 x 2 bed and 3 x 1 bedroom flats), together with associated landscaping, parking, bin and cycle stores.

Date Decision: 07.11.19

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05191/NMA

Ward : **Purley Oaks And
Riddlesdown**

Location : 176 Pampisford Road
South Croydon
CR2 6DB

Type: Non-material amendment

Proposal : Non-material amendment to planning application 17/03118/FUL

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/00126/FUL

Ward : **Purley And Woodcote**

Location : Land At Junction Between Hartley Down And
Coulsdon Road
Coulsdon

Type: Full planning permission

Proposal : Erection of two storey detached dwellinghouse with new vehicular access off Coulsdon Road and parking area with turntable

Date Decision: 07.11.19

Permission Granted

Level: Planning Committee - Minor Applications

Ref. No. : 19/03342/CONR

Ward : **Purley And Woodcote**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 1 Hill Road
Purley
CR8 3AT
Type: Removal of Condition

Proposal : Variation of conditions 1 and 14 of planning permission 18/04955/OUT (Demolition of existing property. Erection of three/four storey building comprising 9 flats (1 x three bedroom, 7 x two bedroom and 1 x 1 bedroom flats) including balconies with parking area, refuse and cycle storage).

Date Decision: 30.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03402/HSE
Location : 82 Foxley Lane
Purley
CR8 3EE
Type: Householder Application
Ward : **Purley And Woodcote**

Proposal : Alterations and erection of a first floor rear extension

Date Decision: 30.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03434/DISC
Location : 95 - 95A Foxley Lane
Purley
CR8 3HP
Type: Discharge of Conditions
Ward : **Purley And Woodcote**

Proposal : Application to discharge condition 4 (Construction Logistics Plan) of planning permission 18/02613/FUL.

Date Decision: 07.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/03765/TRE
Location : 76 Aveling Close
Purley
CR8 4DW
Type: Consent for works to protected trees
Ward : **Purley And Woodcote**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Proposal : The trees are located at the rear of 76-94 Aveling Close

Cutting back all trees, shrubs and bushes to the rear of 76-94 Aveling Close to approx. 20 foot and cutting back any over hanging trees in the back alley.

1no. Hawthorn that is growing into the garage at no.76 to lift away from the garage roof.

Approx. 7no. Ash trees, 2no. Sycamores (these are all young trees) to cut back the limbs by approx. 1.5-2m in line with the hedge leaving a crown spread of approx. 2m. There are only a few limbs on each tree that will need to be cut back.

At the back of house no.80 and 76 there are 2no. Horse Chestnuts to cut back the overhanging from the back gardens, reducing 2no. lower limbs on each tree by approx. 1.5m leaving a 3m crown spread. All the growth will be cut back to good epicormic growth.

Date Decision: 01.11.19

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/03876/FUL

Ward : **Purley And Woodcote**

Location : Flat B, First Floor Flat
5A Russell Hill Parade
Russell Hill Road
Purley
CR8 2LE

Type: Full planning permission

Proposal : Erection of roof extension to create additional storey to enlarge existing flat, associated alterations

Date Decision: 01.11.19

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/04030/HSE

Ward : **Purley And Woodcote**

Location : 5 Oakwood Avenue
Purley
CR8 1AR

Type: Householder Application

Proposal : Loft conversion including rear dormer and increased ridge height and installation of 2 rooflights to the front main roof slope, and 2 rooflights to the front gable roof slopes. [Amended description].

Date Decision: 30.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04069/DISC **Ward : Purley And Woodcote**
Location : 35A Smitham Bottom Lane Type: Discharge of Conditions
Purley
CR8 3DE
Proposal : Discharge of Condition 12 (Construction Logistics Plan) attached to 18/05293/FUL for the demolition of dwellinghouse and erection of 3-storey development containing 9 apartments with associated access, 9 off-street parking spaces, cycle storage and refuse store.

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04070/DISC **Ward : Purley And Woodcote**
Location : 35A Smitham Bottom Lane Type: Discharge of Conditions
Purley
CR8 3DE
Proposal : Discharge Condition 3 (floor levels and sections) attached to 18/05293/FUL for the demolition of dwellinghouse and erection of 3-storey development containing 9 apartments with associated access, 9 off-street parking spaces, cycle storage and refuse store.

Date Decision: 07.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04223/CONR **Ward : Purley And Woodcote**
Location : 49 Pampisford Road Type: Removal of Condition
Purley
CR8 2NJ
Proposal : Variation to Condition 1 (Approved Drawings) of planning permission 18/05362/HSE granted for the erection of single/two storey side and rear extensions, alterations to existing porch to widen and change the roof, conversion of the roof space into habitable accommodation and installation of velux windows on the rear and side elevations.

Date Decision: 31.10.19

Permission Granted

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 12 Downlands Road
Purley
CR8 4JE
Type: Non-material amendment

Proposal : Non-material amendment to planning application 18/04168/HSE

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05265/NMA
Location : 19 Purley Rise
Purley
CR8 3AW
Ward : **Purley And Woodcote**
Type: Non-material amendment

Proposal : Non-material amendment to application 18/03694/FUL

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/01876/HSE
Location : 4 Moir Close
South Croydon
CR2 0LQ
Ward : **Sanderstead**
Type: Householder Application

Proposal : Erection of a single storey rear extension.

Date Decision: 06.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/02233/FUL
Location : 70 Arkwright Road
South Croydon
CR2 0LL
Ward : **Sanderstead**
Type: Full planning permission

Proposal : Demolition of the existing garage and outbuilding to the existing dwelling with alterations to the land levels; Erection of a detached 3 bedroom house with with associated bin, cycle and parking provisions.

Date Decision: 30.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03118/FUL **Ward : Sanderstead**
Location : 3 West Hill **Type: Full planning permission**
South Croydon
CR2 0SB
Proposal : Demolition of existing 2 storey detached house & garage. Erection of a four storey flatted block comprising 9 apartments including associated communal amenity space, landscaping, parking, access, cycle and refuse storage.

Date Decision: 07.11.19

Permission Refused

Level: Planning Committee

Ref. No. : 19/03643/OUT **Ward : Sanderstead**
Location : 34A & 34B Arkwright Road And The Land To **Type: Outline planning permission**
The Rear Of 34 Arkwright Road
South Croydon
CR2 0LL
Proposal : Demolition of existing dwellings. Erection of a three/four storey building comprising 23 flats (6 x 1 bedroom, 14 x 2 bedroom and 3 x 3 bedroom units). Alterations to existing vehicular access/road and creation of parking area, amenity space, cycle and refuse storage

Date Decision: 06.11.19

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/03669/HSE **Ward : Sanderstead**
Location : 1 East Hill **Type: Householder Application**
South Croydon
CR2 0AL
Proposal : Erection of two, 2 storey side extensions, a single storey rear extension and alterations to the front elevation, Alterations to the roof with the introduction of 5 roof dormers.

Date Decision: 08.11.19

Permission Granted

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Date Decision: 08.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04532/HSE **Ward : Sanderstead**
Location : 44 Ewhurst Avenue **Type: Householder Application**
South Croydon
CR2 0DJ
Proposal : Erection of a single storey rear extension, including a raised deck with railings to the rear.

Date Decision: 06.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04559/DISC **Ward : Sanderstead**
Location : 251 Limpsfield Road **Type: Discharge of Conditions**
South Croydon
CR2 9DE
Proposal : Discharge of condition 2 (parking/highways) attached to planning permission 19/01906/HSE for the erection of single storey side extension, relocation of vehicular access and dropped kerb, associated alterations

Date Decision: 01.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04609/HSE **Ward : Sanderstead**
Location : 3 Ewhurst Avenue **Type: Householder Application**
South Croydon
CR2 0DH
Proposal : Erection of a part single, part two storey rear extension.

Date Decision: 06.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04686/LP **Ward : Sanderstead**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 16 Courtlands Close
South Croydon
CR2 0LR
Type: LDC (Proposed) Operations
edged
Proposal : Erection of an outbuilding in the rear garden.

Date Decision: 05.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05089/NMA
Location : 33 Limpsfield Road
South Croydon
CR2 9LA
Ward : **Sanderstead**
Type: Non-material amendment

Proposal : Non material amendment to planning application 18/03136/FUL

Date Decision: 30.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05217/LP
Location : 13 Sundown Avenue
South Croydon
CR2 0RQ
Ward : **Sanderstead**
Type: LDC (Proposed) Operations
edged

Proposal : Erection of hip to gable roof extension with Juliet balcony and roof light.

Date Decision: 08.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05249/LP
Location : 54 Brian Avenue
South Croydon
CR2 9NF
Ward : **Sanderstead**
Type: LDC (Proposed) Operations
edged

Proposal : Erection of single storey side extension, construction of roof lights

Date Decision: 08.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 14 Brent Road
South Croydon
CR2 7NR
Type: Non-material amendment

Proposal : Amendments to 19/01138/FUL to change finish material to smooth render, increase width of roof lights and increase width of windows.

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/02735/FUL
Location : R/O 78 Blenheim Park Road
South Croydon
CR2 6BF
Ward : **South Croydon**
Type: Full planning permission

Proposal : Demolition of 2 existing outbuildings and construction of three storey building with roof accommodation (to front Culmington Road) to form 9 units including 9 associated parking spaces.

Date Decision: 31.10.19

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/03893/GPDO
Location : 172A Selsdon Road
South Croydon
CR2 6PJ
Ward : **South Croydon**
Type: Prior Appvl - Class O offices to houses

Proposal : Change of use of ground and first floors (Use Class B1 - office) to residential use (Use Class C3 - dwelling), to comprise of 3 residential units with associated parking, cycling and refuse storage facilities.

Date Decision: 31.10.19

Approved (prior approvals only)

Level: Delegated Business Meeting

Ref. No. : 19/04217/HSE
Ward : **South Croydon**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 37 Croham Manor Road
South Croydon
CR2 7BJ
Type: Householder Application
Proposal : Erection of a single storey rear extension.

Date Decision: 06.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04246/LP
Location : 10 Deanfield Gardens
Hurst Road
Croydon
CR0 1JU
Type: LDC (Proposed) Operations edged
Ward : **South Croydon**
Proposal : Proposed New Front Porch, Single Storey side and rear extension, Hip to Gable roof extension with rear dormer and two rooflights at front.

Date Decision: 04.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/04265/HSE
Location : 14 Temple Road
Croydon
CR0 1HT
Type: Householder Application
Ward : **South Croydon**
Proposal : Alterations, erection of three rear dormers

Date Decision: 05.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04370/FUL
Location : 101 Avondale Road
South Croydon
CR2 6JF
Type: Full planning permission
Ward : **South Croydon**
Proposal : Demolition of double garage and erection of two storey building comprising 3no. flats (1no. studio & 2no. one bed) with associated landscaping and cycle storage

Date Decision: 08.11.19

Permission Refused

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Level: Delegated Business Meeting

Ref. No. : 19/04391/GPDO
Location : 354 Brighton Road
South Croydon
CR2 6AJ

Ward : South Croydon
Type: Prior Appvl - Class A Larger
House Extns

Proposal : Erection of single storey rear extension projecting out 4.75 metres with a maximum height of 3 metres

Date Decision: 29.10.19

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 19/04946/NMA
Location : 171 Brighton Road
South Croydon
CR2 6EH

Ward : South Croydon
Type: Non-material amendment

Proposal : Non-material amendment to planning application 13/01529/P

Date Decision: 08.11.19

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/03493/DISC
Location : Milton House
50 Milton Road
Croydon

Ward : Selhurst
Type: Discharge of Conditions

Proposal : Discharge of condition 13 (Energy) of permission 17/04901/OUT Erection of a 3 storey building to provide between 20 and 30 residential units, together with associated car parking and landscaping.

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/03729/DISC
Ward : Selhurst

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : Elizabeth House
97 St James's Road
Croydon
CR0 2UU

Type: Discharge of Conditions

Proposal : Details pursuant to condition 3 (datum, landscaping , boundary treatment) in respect to planning permission 18/03944/FUL for the Erection of a 2 storey plus basement residential block with accommodation in the roof to provide 3 flats, associated landscaping, cycle parking and refuse storage facilities

Date Decision: 08.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04210/FUL
Location : Apartment 16, Block A2
226 Whitehorse Road
Croydon
CR0 2LB

Ward : **Selhurst**
Type: Full planning permission

Proposal : Subdivision of one residential unit to create two residential units (1x two bed and 1x three bed) with associated works, including external alterations.

Date Decision: 31.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04312/HSE
Location : 197 Sydenham Road
Croydon
CR0 2ET

Ward : **Selhurst**
Type: Householder Application

Proposal : Erection of part single/part two storey rear extension

Date Decision: 05.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04945/DISC
Location : Ledaire Fabrications
Limes Place
Croydon
CR0 2HA

Ward : **Selhurst**
Type: Discharge of Conditions

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : Addington Golf Club
197-205 Shirley Church Road
Croydon
CR0 5AB
Type: Full planning permission

Proposal : Improvement of existing vehicle accesses to Golf Club to include creating an entrance and exit and erection of walls, railings and pillars

Date Decision: 05.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04081/HSE
Location : 32 Ash Road
Croydon
CR0 8HU
Ward : **Shirley South**
Type: Householder Application
Proposal : Alterations, erection of a single storey ground floor rear extension

Date Decision: 07.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04293/HSE
Location : 23 Eversley Way
Croydon
CR0 8QR
Ward : **Shirley South**
Type: Householder Application
Proposal : Erection of a first floor side extension

Date Decision: 04.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05277/NMA
Location : 2 Pinewood Close
Croydon
CR0 5EX
Ward : **Shirley South**
Type: Non-material amendment
Proposal : Non-material amendment to application 19/02400/FUL

Date Decision: 08.11.19

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04187/GPDO
Location : 1A St Dunstan's Road
South Norwood
London
SE25 6EP

Ward : South Norwood
Type: Prior Appvl - Class B1(c) to Dwelling

Proposal : Use as 4 dwellinghouses

Date Decision: 30.10.19

(Approval) refused

Level: Delegated Business Meeting

Ref. No. : 19/04236/HSE
Location : 15 Camille Close
South Norwood
London
SE25 4EW

Ward : South Norwood
Type: Householder Application

Proposal : Construction of a single storey side extension

Date Decision: 31.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04959/LP
Location : 85 Norhyrst Avenue
South Norwood
London
SE25 4BY

Ward : South Norwood
Type: LDC (Proposed) Operations edged

Proposal : Construction of hip to gable end roof extension; erection of dormer extension in rear roofslope and installation of rooflights in front roofslope.

Date Decision: 01.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : Peak Ignition
Hythe Road
Thornton Heath
CR7 8QP

Type: Full planning permission

Proposal : Alterations, erection of gable dormer windows to roof slopes, and re-build/refurbishment including provision of flat roof to existing rear single storey extension (works in association with prior approval 19/02731/GPDO for Conversion of building for use as 2 two bedroom self-contained flats, provision of associated refuse storage and cycle storage).

Date Decision: 08.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04888/LP
Location : 43 Cranbrook Road
Thornton Heath
CR7 8PQ

Ward : **Thornton Heath**
Type: LDC (Proposed) Operations edged

Proposal : Erection of dormer extension in rear roofslope and installation of rooflights in front roofslope

Date Decision: 01.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/01724/FUL
Location : 6 Haling Park Gardens
South Croydon
CR2 6NP

Ward : **Waddon**
Type: Full planning permission

Proposal : Retrospective planning permission for the conversion of the existing outbuilding into a self contained granny annexe.

Date Decision: 29.10.19

Permission Granted

Level: Planning Committee - Minor Applications

Ref. No. : 19/02576/FUL
Location : 39 Stafford Road
Croydon
CR0 4NG

Ward : **Waddon**
Type: Full planning permission

Proposal : Erection of a first floor rear infil extension to extend the existing kitchen and the erection of a second floor rear extension.

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Date Decision: 30.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03720/HSE **Ward : Waddon**
Location : 28 Goodwin Gardens **Type: Householder Application**
Croydon
CR0 4HS
Proposal : Demolition of garage, erection of two storey side and rear extension to create an annex

Date Decision: 29.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04219/DISC **Ward : Waddon**
Location : 3 Imperial Way **Type: Discharge of Conditions**
Croydon
CR0 4RR
Proposal : Details pursuant to the discharge of conditions 4 (landscaping) and 5 (cycle and bin storage) of planning permission 18/05223/FUL for 'Alterations to the external façade of the building and rationalisation of the site layout, including vehicle parking and access and refuse storage areas in order to provide for the storage and distribution of medical equipment, with ancillary accommodation and facilities for medical assessments, and a showroom.'

Date Decision: 31.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04220/HSE **Ward : Waddon**
Location : 5 William's Terrace **Type: Householder Application**
Croydon
CR0 4HZ
Proposal : Erection of single storey rear extension

Date Decision: 29.10.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04314/TRE **Ward : Waddon**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 51 Old School Place
Croydon
CR0 4GA

Type: Consent for works to protected trees

Proposal : Group 1 x5 Maples and 1x Lime Cut back from the flats side elevation creating a 3m clearance.
Group 2 x two Lime trees - crown lift to 2m measured from ground level (to clear parking spaces)
(TPO no. 7, 1999)

Date Decision: 31.10.19

Consent Granted (Tree App.)

Level: Delegated Business Meeting

Ref. No. : 19/04399/LP

Location : 36 The Ridgeway
Croydon
CR0 4AE

Ward : **Waddon**

Type: LDC (Proposed) Operations edged

Proposal : Erection of single storey rear extension

Date Decision: 05.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/04682/NMA

Location : Trident House
1 Pegasus Road
Croydon
CR0 4RN

Ward : **Waddon**

Type: Non-material amendment

Proposal : Non material amendment to planning permission 18/00042/FUL to include two additional windows, minor changes to the size position and numbers of louvres, replacement lampposts and a new gas intake pipe.

Date Decision: 05.11.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05115/LP

Location : 64 Tanfield Road
Croydon
CR0 1AL

Ward : **Waddon**

Type: LDC (Proposed) Operations edged

Proposal : Loft conversion, erection of L-shaped rear dormer and installation of 2 rooflights in front roofslope.

Date Decision: 08.11.19

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : 86 Watcombe Road
South Norwood
London
SE25 4UZ
Type: Householder Application
Proposal : Erection of single storey side/rear extension
Date Decision: 05.11.19

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04429/GPDO
Location : 53 Belmont Road
South Norwood
London
SE25 4QG
Type: Prior Appvl - Class A Larger House Extns
Ward : Woodside
Proposal : Erection of single storey rear extension projecting out 5.5 metres with a maximum height of 4 metres
Date Decision: 29.10.19

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 19/05168/LP
Location : 51 Enmore Road
South Norwood
London
SE25 5NG
Type: LDC (Proposed) Operations edged
Ward : Woodside
Proposal : Demolition of existing rear extension, erection of single storey rear extension, installation of windows in ground floor side elevation, erection of dormer in rear roofslope and installation of rooflights in front roofslope.
Date Decision: 07.11.19

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/03994/FUL
Location : Croydon University Hospital
530 London Road
Thornton Heath
CR7 7YE
Type: Full planning permission
Ward : West Thornton

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Proposal : Erection of a single storey rear extension which projects out for the rear of the original house by 6 metres with an eaves height of 2.95 metres and a maximum overall height of 3.30 metres

Date Decision: 06.11.19

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 19/04501/DISC

Ward : West Thornton

Location : Dunheved Hotel
639-641 London Road
Thornton Heath
CR7 6AZ

Type: Discharge of Conditions

Proposal : Discharge of Condition 3 (Construction Logistics Plan) of planning application reference: 17/05847/FUL (Rear roof extension at second floor level to provide additional floor space to existing bedrooms to create 4 duplex bedrooms).

Date Decision: 29.10.19

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04999/DISC

Ward : West Thornton

Location : 757-759 London Road
Thornton Heath
CR7 6AW

Type: Discharge of Conditions

Proposal : Discharge of conditions 9 and 10 (WSI) of permission 17/00663/FUL - Demolition of existing Public House and erection of a 5 storey building providing 134sqm of Class A1/A2 use at ground floor, and 6 one bedroom, 4 two bedroom and 3 three bedroom flats above: formation of vehicular access and provision of associated car parking and secure cycle storage.

Date Decision: 08.11.19

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 19/04894/AUT

Ward : Out Of Borough

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 13th November 2019

Location : Volvo Truck And Bus South Ltd
Beddington Farm Road
Beddington
London Borough Of Sutton
CR0 4XB

Type: Consultation from Adjoining
Authority

Proposal : Erection of a single storey front entrance lobby together with external and internal alterations to existing HGV maintenance and repair depot including, provision of 4 new parking spaces, replacement of south boundary treatment fence, recladding of main building and replacement/installation of roller shutter doors, personnel doors and windows

Date Decision: 08.11.19

No Objection

Level: Delegated Business Meeting

This page is intentionally left blank